 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>		PLANNING COMMITTEE
	<b>DATE:</b>		31 <sup>st</sup> October 2018
	<b>REPORT OF:</b>		HEAD OF PLACES & PLANNING
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<b>AGENDA ITEM:</b>	6	<b>WARD:</b>	Salfords and Sidlow

<b>APPLICATION NUMBER:</b>	18/01180/F	<b>VALID:</b>	4 <sup>th</sup> June 2018
<b>APPLICANT:</b>	Goya Developments & Hillwood	<b>AGENT:</b>	PRC
<b>LOCATION:</b>	<b>FORMER PHILIPS RESEARCH LABORATORIES SOUTH SITE, CROSSOAK LANE, SALFORDS</b>		
<b>DESCRIPTION:</b>	The redevelopment of the site to include four employment buildings incorporating 5 units for open b1(b), b1(c), b2 and b8 use comprising 15,623sqm GEA with associated parking and landscape planting.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

## SUMMARY

This is a full application for the redevelopment of the site to provide 5 commercial units with associated parking, service yards and landscaping. The units are proposed to be for open B1(b), B1(c), B2 and B8 use.

The site is within a designated Employment Area, being part of the Salfords Industrial Estate. In this regard, the nature, mix and type of development proposed is consistent with policies and strategy in the Local Plan which specifically seek to direct new industrial, storage and distribution uses to such sites. The proposals would support the aim of the Core Strategy which seeks to make best use of existing employment land within industrial areas and the redevelopment would bring back into use a long vacant brownfield site, consistent with the thrust of the NPPF, bringing economic benefits and job creation in the process.

The layout of the site and design of the buildings are considered to be typical of, and appropriate for, a commercial/industrial estate. Whilst the proposed units would be large in scale and relatively tall (13-15m), this is not unusual for industrial/commercial units and the height would not appear unduly out of keeping in the context of large buildings in the surroundings such as the adjacent Titan Travel offices. The layout of the site, including the fact that the buildings would be generously set back from the road enabling the retention and enhancement of existing boundary landscaping, is considered to help ensure that the buildings would not appear unduly dominant and would maintain the presently verdant

character of the adjoining stretches of Bonehurst Road and Cross Oak Lane. The appearance of the units would reflect modern industrial/warehouse accommodation; however, but would be enhanced somewhat by the addition of areas of complementary timber cladding which add interest to the buildings. Overall, the site is considered to be appropriately designed and is not felt to harm the character of the area.

Whilst the buildings would be relatively large, the separation distances to the nearest residential properties are such that the proposals are not considered to be detrimental to residential amenities with respect to overbearing, outlook or daylighting. The application was accompanied by a Noise Impact Assessment which demonstrates that, subject to the inclusion of an acoustic fence along the southern boundary, the likely use, operation and activity associated with the units would not give rise to an unacceptable noise impact or disturbance to neighbouring properties, either during the day or at night. No other adverse neighbour impacts have been identified.

In terms of access and highways, access to the site would be via an existing access to Cross Oak Lane which would be modified as part of the development. This is considered to be acceptable by the County Highway Authority in terms of visibility and geometry. Specific and detailed consideration has been given to the impact of the movements from the proposed development on the Cross Oak Lane/A23 junction and, whilst it is acknowledged that there would be some impact in terms of additional queuing in the AM and PM peaks, the County Highway Authority concludes that this would not be so severe as to warrant refusal. A condition limiting the amount of floorspace on the development to be used for B8 use is proposed, this would prevent the use of the whole site as a distribution centre which may give rise to different highway impacts. The proposals are considered to provide adequate parking provision for the nature and type of development proposed and the levels of parking are supported by evidence from sites drawn from the national TRICS database. With regard to concerns regarding HGV movements and capacity, the scheme would provide a total of 17 designated HGV loading bays; however, it is concluded that there would be adequate additional space and flexibility within the service yards of a number of the units to allow for HGVs to wait in the event that all loading bays were full. A condition is however recommended requiring the developer to implement double yellow lines on both sides of Cross Oak Lane up to the railway bridge in order to prevent vehicles associated with the site (or any other vehicles) parking on the road which may prejudice highway safety. A Travel Plan and Delivery & Servicing Plans are also recommended to be sought through condition to manage potential impacts on the transport and highway network.

The proposals are considered to satisfy the Sequential Test in respect of flood risk and are considered to be otherwise acceptable in flooding terms. No objection is raised by either the Environment Agency or Surrey CC as the Lead Local Flood Authority. A sustainable drainage system is proposed to be secured by condition.

## **RECOMMENDATION(S)**

Subject to the completion of all documentation required to create a planning obligation under Section 106 of the Town and Country Planning Act 1990 (as amended) to secure:

- (i) £3,000 towards a review and implementation of parking restrictions for up to five years past full occupation of the site;
- (ii) £6,150 towards Travel Plan monitoring and auditing
- (iii) £4,000 towards reviewing the rail bridge height restriction east of the site on Cross Oak Lane
- (iv) The Council's legal costs in preparing the agreement;

Planning permission is **GRANTED** subject to conditions.

In the event that a satisfactorily completed obligation is not received by 31 December 2018 or such longer period as may be agreed, the Head of Planning be authorised to refuse permission for the following reason:

The proposal fails to make adequate provision for the monitoring of sustainable travel measures and local highway impacts and therefore could give rise to a situation prejudicial to highway safety or which would fail to promote sustainable travel, contrary to policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and Policy CS17 of the Reigate and Banstead Core Strategy 2014.

## Consultations:

County Highway Authority: No objection subject to conditions. Comments as follows:

*"The model has been audited by SCC modellers. The model assumes that the development would be used for the proposed mix of development and has been revised according to instructions from our modelling team in such a way that the inputs and assumptions made have all been passed. The model shows that the development would add more traffic such that the junction would be over its theoretical capacity. However no new junctions would be affected. In the AM peak queues on the north and south sides of the A23 Bonehurst Road junction with Cross Oak Lane would increase from respectively 145 metres and 165 metres in 2023 without the development to 163 metres and 175 metres with the development. In the PM peak queues on the north and south sides of the A23 Bonehurst Road junction with Cross Oak Lane would increase from respectively 174metres and 156 metres in 2023 without the development to 201 metres and 178 metres with the development.*

*The queues on Cross Oak Lane will increase but they will not be so long as to reach the railway bridge east of the access. The only junction that would be affected would be the new junction on to the A23 from the Horley North West Sector. However that junction would be affected anyway because queues already form where the junction is proposed to be located. No additional junctions north of the new junction would be affected by the longer queues in either the AM or PM peaks.*

*I have carried out a sensitivity test assuming that the proposed development would comprise 9000m2 of B8 Distribution use. I have assumed the rest of the site would be used as per the proposed B1c and B2 uses. This sensitivity test shows that such a use would not increase queue lengths to such an extent that new highway junctions would be affected.*

*The developer has carried out a parking accumulation survey using site from TRICS to establish whether adequate parking for vehicles including HGV vehicles is being proposed. The data from the developer shows that adequate parking is being proposed for the mix of land use proposed. However to safe guard all highway users against future demand for parking on the highway I have recommended that the developer provide double yellow lines on both sides of Cross Oak Lane between the rail bridge east of the eastern most access and the Cross Oak Lane junction with the A23. In addition I have asked for a contribution of £3000 towards reviewing the parking restrictions on Cross Oak Lane east of the rail bridge should this become necessary after occupation of the development for a period of up to 5 years post full occupation of the proposed development.*

*If the site were used by a B8 distribution company, there is likely to be more demand for parking. However the developer is proposing a quantum of parking spaces that would more than cover the likely demand for car parking therefore leaving unfilled spaces. A proportion of those spaces within the site could be used for parking of HGVs while the driver waits for a loading/unloading bay. This would not displace cars onto the highway because the parking accumulation shows that there would be adequate space within the development to accommodate demand for parking of HGVs and non HGVs.*



*The developer is proposing to alter the existing western most access and to close the eastern most access. Both of these are subject to conditions to be carried out in accordance with plans to be submitted. With respect to the modified access, there is adequate visibility proposed. Turning overlays show that the access would have adequate geometry to accommodate HGVs albeit with some crossing into the opposing traffic lane. In order to prevent parking on Cross Oak Lane I have recommended a revised plan showing details of double yellow lines. These details can be provided as part of highway works under Section 278 of the Highways Act 1980.*

*I have recommended a condition for details of a Construction Transport Management Plan to be submitted. The developer has submitted a travel plan, but this needs to be revised."*

Contaminated Land Officer: Identifies potential for ground contamination to be present on and/or in close proximity to the site and therefore recommends conditions.

Environment Agency: No objection subject to conditions

Surrey Lead Local Flood Authority: No objection subject to conditions

Surrey County Council Minerals & Waste Policy Team: No comments

Natural England: No comments

Salfords and Sidlow Parish Council: Objects on the basis of four main concerns: a) density of units, no room to accommodate likely number of heavy and light goods vehicle movements on the proposed site; b) traffic that would be generated, including cumulative effects with other developments and capacity of junctions onto the A23 to cope with additional traffic; c) inadequate parking and d) height of the building could be overbearing. Also raises additional concerns regarding noise and disturbance, including night time activities.

Horley Town Council: Objects on the basis that the current transport infrastructure will not support the proposal at both Crossoak Lane and at the junction of Crossoak Lane with the A23, especially when taking into account that the Westvale Park development access road joins into this junction. Supports development of the site but suggests that offices/residential would be more appropriate.

Gatwick Airport: Recommends condition requiring a bird hazard management plan

NATS: No objection with respect to National Air Traffic Services safeguarding criteria

Network Rail: Recommends informative notes regarding practices which the developer must follow, during construction and after completion of works on site, to ensure there would be no prejudice to the operation and safety of the railway.

## **Representations:**

Letters were sent to neighbouring properties on 13<sup>th</sup> June 2018; a site notice was posted 3<sup>rd</sup> July 2018 and the application was advertised in local press on 28<sup>th</sup> June 2018.

One response was received which neither supported or objected to the proposals but expressed pleasure that the proposed site layout includes for retention/relocation of the Polar Theme statue

## **1.0 Site and Character Appraisal**

- 1.1 The site comprises the site of the former Philips Laboratory, situated on the eastern side of the A23 and on the southern end of the urban area of Salfords. The site is cleared, with the majority of the previous buildings having been demolished to slab level back in 2009/10 and the final remaining building (Building J) being demolished earlier this year under a separate demolition prior approval. The site is part of the designated Salfords Industrial Area within the 2005 Borough Local Plan.
- 1.2 The southern boundary of the site marks the transition between the urban area of Salfords and the Metropolitan Green Belt. To the south of the site, is an area of open amenity land/recreational space and a small number of residential properties, all of which are in the Metropolitan Green Belt. The site adjoins the Brighton mainline railway line to the east which is elevated up an embankment. To the north is an office campus of Titan Travel, which is also part of the Salfords Industrial area. Land on the opposite side of the A23 is in the Green Belt also.
- 1.3 The site accesses onto Cross Oak Lane, close to the signalised junction with the A23. To the east, is a restricted height railway bridge. There are trees on most boundaries of the site, notably the boundaries with the A23 and Cross Oak Lane and this is a feature which prevails along much of this stretch of the A23. The northern part of the site is within Flood Zones 2 and 3 according to EA Flood Maps.
- 1.4 As a whole, the application site extends to approximately 3.12ha.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: Pre-application advice was sought prior to submission of the application. Advice was given in respect of the sensitivity of the highways issues and the need for this to be robustly evidenced and justified in any application. The proximity of the buildings and car parking to the road frontage was identified as a concern and more generous separation and landscaping was encouraged. No in principle objection was raised given the site is a designated employment area.
- 2.2 Improvements secured during the course of the application: Further amendments to the layout to increase the set back of Unit 1 from the corner of A23/Crossoak Lane and to set back areas of parking from Crossoak Lane with associated minor reduction in amount of floorspace (c.208sqm).
- 2.3 Further improvements to be secured through planning conditions or legal agreement: Various conditions are recommended regarding highways and access works, including the implementation of restrictions along Crossoak Lane. Conditions are also recommended to secure implementation of the landscaping, restoration and relocation of the Polar Theme statue and compliance with noise mitigation

recommendations. A legal agreement is proposed to secure contributions towards travel plan monitoring, parking review and bridge signage.

### **3.0 Relevant Planning and Enforcement History**

3.1 There is extensive planning history associated with the historic use and development as an employment site. This includes the following which are most recent/relevant:

- 93/02280/F – Erection of new research building and additional car parking – Approved
- 00/04690/F - Research building, storage, workshops, laboratories & offices – Approved with conditions
- 09/00822/CU - Change of use of building to use class B8 storage or distribution – Approved with conditions

### **4.0 Proposal and Design Approach**

4.1 The proposed development seeks planning permission for the redevelopment of the site to provide four buildings consisting of 5 units for open B1(b), B1(c), B2 or B8 use.

4.2 The units would be laid out around a central access road, with three on the western side adjacent to the frontage with the A23 and a further two alongside the embankment with the railway line. The units would range in size from 1,923sqm to 4,702sqm, with the units along the A23 broadly 13.6m in height and the units adjacent to the railway line approximately 15.1m. Each unit would have a large open plan warehouse type space with ancillary mezzanine office accommodation. The building would employ a mix of profiled metal cladding and timber cladding to the main elevations, with profiled metal roof.

4.3 The development would use the existing western access to the site which would lead to the main access road. Units 4 and 5 would have their own dedicated secure yards serving the HGV loading bays and providing some vehicle parking. These units would also have separate parking areas. Units 1, 2 and 3 would have smaller loading bay areas, served directly off the main access road. Each unit would have additional surface parking. New planting is proposed along the northern, eastern and southern boundaries of the site.

4.4 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.

4.5 Evidence of the applicant's design approach is set out below:

Assessment	<p>The Design &amp; Access (D&amp;A) Statement describes that the existing site as being generally regular in shape and relatively level. The site covers the former laboratory site, which predominantly remains as vacant land; the previous buildings were demolished late 2009. The site is part of the Salfords Industrial Estate which is fairly linear and is split in two, separated by a residential area. The application site is bounded by Bonehurst Road to the west and is tree line with extensive hedgerow. The northern boundary along Cross Oak Lane is similarly tree lined and there is a ditch along the boundary which is culverted at the two access points. The Metropolitan Green Belt surrounds the application site, including the former PRL sports ground to the south of the site. Existing buildings surrounding the proposed development range between circa 7.5-13m in height to the ridge.</p> <p>The trees that surround the application site offer an important buffer to surrounding Metropolitan Green Belt to the east, south and west. The proposal sets the building back from these to protect them.</p>
Involvement	<p>The Planning Statement identifies that pre-application advice was sought from the Council in 2018 and design of the scheme amended in response. No evidence of public consultation is provided in the submission.</p>
Evaluation	<p>The Statement sets out the evolution of the design of the scheme, as a result of the pre-application discussions. This includes reducing addressing the set back and massing onto the A23 and the landscaping of the site. No evidence of other development options considered is identified within the applicant's submission.</p>
Design	<p>The Design Statement explains that the chosen design seeks to make best use of the site to provide a commercially viable redevelopment but whilst also not being overly dense so as to limit the functionality of the buildings. The extent of yard, loading doors, office content and building height have been balanced to suit commercial requirements. The scheme has been designed so that the buildings front onto the service road. The positioning of first floor office accommodation seeks to allow for natural surveillance of the site as well as acting as a feature onto the road frontages. The scheme incorporates additional landscape buffer and boundary treatments to reduce impacts to the Green Belt and residential uses to the south. Whilst the size of the buildings is market orientated, the layout is design lead, siting the larger scale and taller units along the railway line.</p>

#### 4.6 Further details of the development are as follows:

Site area	3.1ha
Existing use	Cleared site (previously Research & Development offices/laboratory)
Proposed use	Mixed industrial/distribution (open B1(b), B1(c), B2 and B8)
Number of units	5
Total floorspace	15,623sqm
Proposed parking spaces (exc. HGVs)	229
Parking standard	BLP 2005 – ranges from 521 maximum (if all B1/B2) to 156 (if all B8 storage)

## 5.0 Policy Context

### 5.1 Designation

Urban Area  
Employment Area  
Part Flood Zone 2 and Flood Zone 3

#### Reigate and Banstead Core Strategy

CS1 (Presumption in favour of sustainable development)  
CS4 (Valued townscapes and historic environment)  
CS5 (Valued people/economic development)  
CS8 (Area 3: Horley)  
CS10 (Sustainable development)  
CS11 (Sustainable construction)  
CS12 (Infrastructure delivery)  
CS17 (Travel options and accessibility)

### 5.2 Reigate & Banstead Borough Local Plan 2005

Employment	Em1, Em2, Em3, Em8
Movement	Mo3, Mo4, Mo5, Mo6, Mo7
Utilities	Ut4

### 5.3 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Supplementary	Developer Contributions SPD
Planning Guidance	Local Distinctiveness Design Guide

Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010 (as
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amended)  
Conservation of Habitats and Species Regulations 2017  
Public Sector Equality Duty

## 6.0 Assessment

- 6.1 The application site comprises a cleared site formally used as a research and development campus. The site is within a designated Employment Area within the 2005 Borough Local Plan and is partially within Flood Zone 2 and 3 at its northern end.
- 6.2 The main issues to consider are therefore:
- principle of development
  - design and impact on the character of the area
  - effects on the amenity of neighbouring properties
  - access, parking and highway implications
  - flooding and drainage
  - other matters

### Principle of development

- 6.3 The application site is within a designated Employment Area, namely Salfords Industrial Estate which occupies a ribbon along the eastern side of the A23. The proposals seek to redevelop the site for to provide 5 commercial units, totalling 15,623sqm, with an open B1(b), B1(c), B2 or B8 use. Given the designation of the site and the proposed development, policies Em2 and Em8 of the Local Plan 2005 and policy CS5 of the Core Strategy are particularly relevant.
- 6.4 Policy CS5 seeks, in general terms, to promote sustainable economic prosperity in the Borough, setting out that the Council will plan *“for a range of types and sizes of employment premises to cater for the needs of established, growing and start-up businesses”* by *“focussing on retaining and making best use of existing employment land, particularly within town centres and industrial areas”*. These proposals, which would redevelop a large site within a designated industrial area which has lain vacant for a number of years, would be wholly consistent with the thrust of this policy. The proposals would make good use of a previously developed site which the NPPF advises should be given “substantial weight”. The Borough Local Plan specifically encourages and seeks to direct the sorts of industrial, storage and distribution uses proposed in this application to designated Employment Areas (which this site is). The proposals would support this overarching strategy.
- 6.5 Turning to the other Borough Local Plan policies, policy Em2 sets out the need to have consideration to a number of factors in considering proposals for industrial, storage and distribution uses. Limb (ii) of this policy requires that consideration be given to whether there are alternative land/premises available to deliver the proposed employment provision, in essence, seeking to manage supply to avoid undue pressure on housing and labour markets. It is questionable, whether this limb applies to this site (as the policies specifically exempts the *“redevelopment of outworn industrial storage and distribution uses”* from this test) and furthermore, it is questionable whether such a restrictive approach is consistent with the Framework

which provides that *“decisions should help create the conditions in which businesses can invest, expand and adapt”*.

- 6.6 At any rate, the Council's latest Commercial Development Monitor demonstrates that there has been little, if any, growth in industrial floorspace in the borough over the past decade and, furthermore, indicates that current extant planning permissions would actually result in a net loss of industrial and distribution space if implemented (6,230sqm). According to the same monitor, the level of available (i.e. on the market to buy or lease) industrial and warehouse space identified through the Council's monitoring has fallen by around half in the past 5 years (29,000sqm in 2013 to 14,191sqm in 2018). The Council's Economic Prosperity team advises that their engagement with the market supports the view that there is a demand for the type and size of accommodation proposed in the borough, with limited opportunities available elsewhere. Furthermore, it should be noted that the Core Strategy (Policy CS8 Area 3) anticipates delivery of 24,000sqm of employment provision in the Horley/Salfords area; redevelopment of this site for the scale of floorspace proposed would support this and, given limited delivery to date, would not result in this figure being exceeded. As such, there is not considered to be an unacceptable risk of pressure in labour or housing markets. Taking all of the above into account, it is concluded that the proposals would pass limb (ii) of Policy Em2. The other aspects of Em2 (e.g. in respect of infrastructure impacts and housing/environmental policies) are discussed below.
- 6.7 With regard to policy Em8, the other specific provision this introduces relates to a general desire to resist regional distribution centres. Whilst this proposal provides a total of 15,623sqm (thus exceeding the 5,000sqm set out in the policy), this would be split across 5 units with no individual unit exceeding 5,000sqm (the largest would be 4,602sqm). The proposal does not therefore conflict with this requirement. This approach is consistent with other developments in the Salfords Industrial Area exceeding 5,000sqm in total but split amongst smaller units which have been approved.
- 6.8 Taking all of the above into account, the proposed uses on this designated Employment Area are considered, in principle, to be wholly consistent with development plan policies. Furthermore, the proposals would make good use of a vacant previously developed site of generally low environmental value which the Framework advises should be given substantial weight.

#### Design and impact on the character of the area

- 6.9 The proposals were subject pre-applications discussions with Officers regarding the proposed scale, massing and design of the building. The design has, as set out in the applicant's Design & Access Statement, evolved significantly through this process and the application.
- 6.10 The layout is considered to be typical of, and suitable for, a modern industrial/commercial estate. The five units would be laid out around a broadly central access road running north-south through the site, each with its own yard/loading area and dedicated parking, either to the side or front of the building. The units would front onto the access road providing natural surveillance and

creating a frontage. Some tree planting and soft landscaping would be incorporated within the parking areas and along the access road, the extent of this is considered to be appropriate given the active commercial environment of the estate (where robustness is important) and mindful of the extensive landscape setting which would be retained around the boundaries of the site.

- 6.11 Whilst the proposed units would be large in scale and relatively tall (13-15m), this is not unusual for industrial/commercial units given their function. Furthermore, as the submitted street scene drawings demonstrate, the height of the buildings would not be dissimilar to the adjacent Titan Travel office building, and whilst there would be a marked step down in scale to properties to the south and west, there would be sufficient separation such that this would not appear stark or abrupt.
- 6.12 In addition, the layout of the site is considered to mitigate their impact on the character and street scenes of the wider area. The units would all be significantly set back from the road, retaining a gap of between 17.5 and 25m to Cross Oak Lane and 15 to 18m from Bonehurst Road (A23), helping to ensure that they would not appear unduly dominant in views along these roads. The existing boundary tree and hedgerow cover would also be retained along both frontage, and would be extensively supplemented along Bonehurst Road, to ensure that the verdant, "parkway" character of these thoroughfares would be maintained. The layout also position the larger footprint, taller units towards the back (east) of the site along the railway line where their scale would be less appreciable in the public realm and where it would be read against the backdrop of the steeply rising land of the railway embankment. Landscaping would also provide screening along the southern boundary in order to respect the transition to the adjoining countryside.
- 6.13 In terms of form and appearance, the units would be relatively typical of commercial/industrial premises with a simple, uncomplicated boxy form with shallow pitched roofs. Each of the units would have mezzanine office accommodation and the configuration/internal layout seeks to position these office elements so as to provide a feature onto more prominent frontages. In terms of materials, the buildings would be predominantly clad in profiled metal; however, the design also incorporates areas of timber cladding to good effect to break up and add visual interest to the elevations whilst also aiding legibility by demarcating entrances/office areas. Timber cladding is also proposed at high level on the elevations facing Bonehurst Road to provide a more natural backdrop behind the canopy of the trees along this frontage.
- 6.14 Overall, the layout of the site and design of the buildings is considered to be acceptable with respect to its end use and impact on the character of the surrounding area. The proposal would therefore comply with policy Em3 of the Local Plan 2005, CS4 and CS10 of the Core Strategy and the provisions of the Framework in respect of achieving well designed places.

#### Effects on the amenity of neighbouring properties

- 6.15 The nearest residential neighbours to the development site are a bungalow to the south (20 Bonehurst Road) and on the opposite side of the A23 (including Horley Place which has consent to convert to residential). Further residential properties



exist to the north, beyond the Titan Travel campus (Empire Villas); however, these are approximately 140m from the site.

- 6.16 In terms of 30 Bonehurst Road, the nearest of the proposed buildings would be over 35m from the side boundary of this neighbour. At this distance, whilst acknowledging the scale and height of the proposed buildings, it is not considered that it would be unduly overbearing nor give rise to unacceptable overshadowing (particularly the application site is due north of this neighbour). The buffer afforded by the existing and proposed tree cover between unit 3 and no.30 would also assist in screening the building so it would not appear obtrusive. No windows are proposed on the southern elevation of Unit 3; hence there is not considered to be any risk of overlooking or loss of privacy.
- 6.17 In terms of Horley Place and the other residential units on the opposite side of Bonehurst Road, these would similarly be well separate from the proposed units (c.45m). Given this distance, and the presence of the intervening major road, it is not considered that the buildings would appear dominant or unduly harmful to the outlook of the existing and potential residences on this side of Bonehurst Road. The proposals would pass the 25 degree rule in relation to easterly facing windows on these properties and thus are not considered to cause harmful loss of light.
- 6.18 The proposed development would introduce a considerable level of activity and permission is sought without any limitation on working hours. Whilst this is not objectionable in principle (and other similar developments have been granted free of restriction), consideration needs to be given to the impact of potential disturbance on neighbouring properties. The application was supported by a Noise Impact Assessment which considers both daytime and night-time noise arising from the proposals. The scope of the assessment, in terms of the types of activities which it factors into the analysis and the level of noise generated is considered to be appropriate and a reasonable representation of the potential operations on site (e.g. it includes activities within the buildings, plant and machinery on the buildings and operating outside and the noise from vehicular movements including idling vehicles).
- 6.19 The assessment concludes that, in respect of properties opposite the site on the western side of Bonehurst Road, the assessed level of noise (at 43dB LA<sub>eq1hr</sub>) they would experience as a result of the proposal would be below the daytime background noise level by 18dB and, for the night-time scenario would be 39dB which is equal to the background level. On this basis, the proposals would not create an unacceptable level of noise or disturbance for these neighbours. With regards to the single dwelling to the south at no.30 Bonehurst Road, the ambient noise levels are calculated as 52 dB during the day and 35 at night. Initial calculations showed that excesses of acceptable noise levels would occur for this neighbour; and, as a result, a 4m acoustic fence/noise barrier has been designed into the scheme along the majority of the southern boundary. When the scheme is remodelled with this in place the predicted noise levels of 45dB LA<sub>eq1hr</sub> during the day (which is below the background level of 52dB) and 35dB at night (i.e. no different to the background noise). Therefore, with the acoustic measures proposed, the proposals would not cause an unacceptable noise impact on no.30 Bonehurst

Road. A condition requiring implementation of the acoustic fence (and submission of details of its performance) is recommended.

- 6.20 The Noise Impact Assessment incorporates provision for mechanical plant within its calculations. However, as the units are to be speculatively built, the final plant requirements are not fully known at this point. To safeguard the amenity of neighbours, a condition requiring details of all plant and machinery required for each unit prior to its occupation is recommended. This will provide the Council scope to ensure that any plant would not have an unacceptable noise implication.
- 6.21 An External Lighting proposal was accompanied with the application, detailing all external lighting (e.g. to access roads/service yards). The strategy includes lighting diagrams which demonstrate that, at the immediate southern boundaries of the site, lux levels would be a maximum of 23 in isolated locations but generally in the low teens. These light levels are not considered to give rise to unacceptable light pollution or disturbance to no.30 Bonehurst Road, particularly given the significant intervening boundary planting along this southern boundary which would screen and dapple any light. The plans do not show any lighting along the western boundary.
- 6.22 Overall, whilst the proposal would result in some change for neighbouring properties, given the generous separation distances, intervening tree cover and proposed acoustic measures, it would not result in an unacceptable impact on amenity or living conditions of neighbouring occupiers. The proposal therefore complies with policy Em3 of the Borough Local Plan 2005 in this respect.

#### Flooding and drainage

- 6.23 According to Environment Agency flood maps, the northern part of the site closest to Cross Oak Lane is partially within Flood Zone 2 and partially within Flood Zone 3.
- 6.24 With regard to the Sequential Test, as above, the current proposals would contribute to meeting the 24,000sqm of additional employment floorspace anticipated for the Horley/Salfords area in the Core Strategy. Given the policies in the Local Plan which seek to direct industrial/warehouse development to designated Employment Areas and the limited availability of land elsewhere in such estates to accommodate a development of the nature proposed in this application, it is concluded that the proposals could not be achieved on land at lower risk of flooding. There are also specific visual and physical benefits associated with regenerating and redeveloping this prominent and long vacant site which would clearly not be achieved if development was carried out elsewhere. Consequently, the proposals satisfy the Sequential Test.
- 6.25 In terms of the flooding risk on the site, the new buildings are sited such that they would, as far as possible, avoid development within Flood Zone 2 and there would be no development within Flood Zone 3. Given this, it is not considered necessary for the development to satisfy the exception test.
- 6.26 The application was supported by a Flood Risk Assessment and initial drainage strategy. This has been reviewed by both the Environment Agency – who raises no

objection to the proposals from a flood risk perspective. In respect of drainage, the submission was reviewed by Surrey County Council as the Lead Local Flood Authority. The LLFA initially raised concerns regarding the proposed drainage strategy; however, following dialogue and agreement from the applicant to increase storage to reduce run-off rates, the LLFA has confirmed they have no objection subject to conditions. Compared to the historic situation on site (and historic significant extent of hardstanding), this would likely represent a marked improvement in the management of surface water from the site.

- 6.27 Based on the above and taking account of the expert advice of the relevant consultees, it is concluded that the application passes the Sequential Test and, furthermore, would respond to the flood risk on site appropriately in terms of drainage provisions. On this basis, the proposal complies with Policy CS10 of the Core Strategy, Ut4 of the Local Plan and the relevant national policy provisions.

Accessibility, parking and highway implications

- 6.28 The development would be access from Cross Oak Lane utilising a current access which would be brought back into use and appropriately modified/improved. The second of the two existing accesses (closer to the railway bridge) would be closed and the kerb/footway/verge reinstated.
- 6.29 The application was accompanied by a Transport Assessment which examines the travel patterns, parking demand and trip generation which would be associated with the proposed development. During the course of the application, the developer also provided – at the request of the County Council – specific modelling of the potential impacts of the proposals on nearby junctions, including the A23 with Cross Oak Lane and the proposed junction between the A23 and the new link road to Westvale Park. This modelling was undertaken and adapted in accordance with recommendation from the County Council's modelling team and as such the inputs and assumptions made are all agreed by the County Council.
- 6.30 The results of this modelling show that in 2023, queues in the AM peak on the north side of the A23 junction with Cross Oak Lane would increase from 145m (without development) to 163m with the development. Queues on the south side of the junction would increase from 165m to 175m. In the PM peak, the increases would be 174m to 201m (north side) and 156m to 178m (south side). Overall, given the volumes of traffic on the A23, these increases in queue lengths are considered to be relatively modest and it is concluded that they do not represent the sort of "severe" impact on the operation of the junction and wider highway which the NPPF advises would warrant refusal. The County Highway Authority particularly notes that these additional queues would not affect any new junctions compared to the existing situation (other than the proposed new A23/Westvale Park junction which would be affected at any rate). The queues on Cross Oak Lane would similarly increase; however, they would not reach as far back as the railway bridge hence the impacts are not considered to be unacceptable or prejudicial to highway operation.
- 6.31 The TRICS comparator sites which underpinned the above modelling were industrial commercial estates with a mix of industrial and warehouse/distribution uses. The effect of a wholly B8 use of the site (i.e. as a distribution centre) has not

therefore been fully modelled. On this basis, a condition to limit the amount of floorspace on the development which could be used in B8 use is considered reasonable and necessary. The County Highway Authority has analysed the evidence and considers a limit of 9,098sqm is reasonable and would be likely to result in queue lengths ; this would effectively mean that no more than 3 of the proposed units could be operated in B8 use (and if the two largest units are in B8 use then no others).

- 6.32 As above, the site would be accessed using an existing site access to Cross Oak Lane which would be re-opened and modified. The County Highway Authority has reviewed the access point and considers that it would provide adequate visibility. The CHAs response also concludes that, whilst tracking overlays show that a HGV exiting the site in a westerly direction would have to cross partially into the opposing traffic lane, the access point would have adequate geometry to accommodate HGVs, particularly mindful of the fact that the relatively low numbers of HGV movements expected at peak times (as per the TRICS analysis carried out by the applicant and County Council) and given the proximity to a signalised junction which will provide periods of no flow eastbound along Cross Oak Lane providing space for HGVs to turn out of the site without impeding oncoming vehicles. The County Highway Authority response does however acknowledge that any on-street parking along Cross Oak Lane could affect this situation (as it would narrow the carriageway), hence, a condition is recommended to require the developer to implement double yellow line parking restrictions along both sides of Cross Oak Lane between the A23 and the rail bridge. Given the circumstances, this is clearly necessary and reasonable.
- 6.33 Concerns have been raised regarding the adequacy of the internal layout, particularly in terms of manoeuvrability of HGVs. In this regard, the applicant has provided tracking/swept path drawings for the two self-contained yards (Units 4 and 5) which demonstrate that HGVs can manoeuvre satisfactorily into each of the loading bay positions without conflict with any other vehicles (including parked vehicles) within the yards. Whilst it is acknowledged that the yards are tight, they are therefore adequate and simply represent a design which seeks to maximise the efficient use of the site.
- 6.34 The scheme incorporates a total of 229 vehicle parking spaces (excluding HGVs). The standards in the Borough Local Plan 2005 would advise anywhere between 156 and 521 spaces; however, it should be noted that the larger figure assumes that all of the units would be used for B1 (b) research and development which is considered unlikely to be a realistic scenario given the nature and size of the units. At any rate, the standards in the Borough Local Plan are maximum. Using evidence from the national TRICS database (and based on comparator sites specifically suggested by the County Council), the applicant's transport evidence demonstrates that likely peak parking accumulation is likely to be 149 vehicles; hence, the parking provision proposed would provide ample capacity with a considerable surplus of c.75 vehicles. Subsequent analysis by the County Council assuming the whole site was operated in B8 use suggests that peak parking accumulation (all vehicles) would reach 189 vehicles, still some distance below the 229 spaces proposed. On this basis, the general level of parking within the site is considered adequate and unlikely to give rise to displacement parking on Cross Oak Lane.

- 6.35 With regards to HGVs, the development incorporates a total of 17 loading bays across the 5 units. The applicant's initial analysis suggested a maximum of 72 total HGV trips to and from the site between 7am and 7pm, with no more than 2 loading bays ever occupied in any given half hour period. Concerns were expressed in local representations, including from Salfords and Sidlow Parish Council, about the robustness of this analysis.
- 6.36 As a result, further independent investigations were carried out by the County Highway Authority using TRICS data assuming sites of a similar size (15-20,000sqm) in use entirely as a distribution centre. These sites show a maximum HGV accumulation of 23 (based on c.270 HGV arrivals throughout the day). This would potentially exceed the number of loading bays on the proposed scheme by 6. However, as above, it is intended through condition to limit the amount of floorspace which could be used as B8 space on this site (to c.9,000sqm) such that these level of accumulations (which are based on 15-20,000sqm B8 distribution uses) are unlikely to arise.
- 6.37 Furthermore, it should be noted that this peak accumulation is anticipated to occur late at night (23:30-24:00), when general vehicle parking accumulation for such developments is shown to be lower (and at any rate, as discussed above at paragraph 6.34, there would be a surplus of normal vehicle parking spaces in such a scenario even at peak accumulation). In this regard, there are considered to be adequate areas within the site where any excess HGVs could be "stacked" whilst awaiting access to a loading bay, particularly – for example – in the yards of Units 1, 4 and 5 and - in a worst case - along the internal estate road. With this evidence in mind, there is considered to be adequate space within the site to accommodate and manage HGV movements such that there would be unlikely to be any overspill on surrounding roads (e.g. Cross Oak Lane).
- 6.38 However, to safeguard this position, the County Highway Authority has also recommended – as above – that a condition be imposed requiring the developer to implement double yellow lines on both sides of Cross Oak Lane between the A23 and railway bridge to further prevent HGVs parking this road whilst waiting to enter. A further condition is also recommended to require the submission and approval of a Delivery & Servicing Plan setting out how the site will be operated and managed (e.g. in terms of delivery sequencing/timing, management of parking areas, etc.) to prevent overspill HGV parking on surrounding roads. With these additional measures, it is concluded that the likely HGV activity associated with the site would not prejudice safety or operation of surrounding roads.
- 6.39 The application was supported by a draft Travel Plan designed to encourage sustainable travel to and from the site, particularly for staff and visitors. The County Council has recommended that a final revised version be secured by condition prior to occupation; this is considered necessary to ensure that the development would comply with Policy CS17.
- 6.40 Subject to the conditions proposed by the County Highway Authority, the proposal is considered to comply with policies Em3, Mo4, Mo5, Mo6 and Mo7 of the Borough Local Plan and policy CS17 of the Core Strategy.

Community Infrastructure Levy (CIL) and requested contributions

- 6.41 The proposal, being for new industrial and distribution premises, falls outside of the uses which attract a charge based on the Council's adopted Charging Schedule and as such the development would not be liable to pay CIL.
- 6.42 The Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 and state that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence including costed spending plans to demonstrate what the money requested would be spent on.
- 6.43 In this case, as above, the County Council has requested contributions towards a future parking review to the east of the rail bridge on Cross Oak Lane (£3,000), revised signage on the Cross Oak Lane rail bridge (£4,000) and travel plan monitoring (£6,150). These contributions are considered necessary to address and avoid potential unacceptable highways impacts and are proportionate in their scale and kind to the development proposed. A clear justification for each has been provided by the County Highway Authority and the contributions have been accepted and agreed by the applicant. These will be secured through a legal agreement. In addition, the County Highway Authority has requested that the applicant carry out double yellow lining along both sides of Cross Oak Lane between the A23 and the railway bridge; however, this can be secured through condition and subsequent s278 works.

Other matters

- 6.44 The application was supported by appropriate arboricultural surveys and assessments which have been reviewed by the Council's Tree Officer. The Tree Officer concludes that the proposed development will not result in significant loss of mature trees and vegetation and notes that the new landscaping and tree planting proposed will *"not only mitigate the minor tree loss but will also provide significant enhancements and improvements to the existing landscape for the long term"*. In respect of the tree protection and method statements provided, the Tree Officer advises that a finalised Tree Protection Plan and Arboricultural Method Statement (including provision for a pre-commencement meeting and monitoring) should be required prior to commencement.
- 6.45 The application site is not subject to any specific nature conservation designations. A Preliminary Ecological Appraisal was supplied with the application which concludes that the habitats within the site are generally of limited ecological value (particularly owing to the fact that much of the site is hardstanding); however, the site does provide habitat which could support reptiles and breeding birds. Given the general limited ecological potential, no further surveys are recommended however the report makes recommendations regarding working practices and ecological

enhancement measures. These are agreed and a condition requiring compliance with the measures in the report is recommended.

- 6.46 The application was supported by an Energy Statement which discusses the measures to be used on site to achieve a reduction in energy use and associated emissions, focussing on a passive enhancement measures (i.e. building performance to reduce demands for heating/cooling) and active measures (such as LED lighting and switching) to minimise energy use. The Statement also acknowledges the need for the scheme to comply with BREEAM 'Very Good' standard as per Policy CS11. A condition requiring BREEAM compliance will be imposed.
- 6.47 The Conservation Officer has identified that the sculpture which was previously on site (but was removed for protection as part of a condition for the prior approval of the demolition) is by noted sculptor Keith Godwin and is known as the "Polar Theme". The Conservation Officer considers the sculpture to be an important and valuable asset which he considers worthy of listing. Whilst it is therefore a non-designated heritage asset at present, it is considered to be of greater than local significance and interest. A condition to secure, where feasible, repair and restoration of the sculpture to enable its relocation within the site in order to preserve this element of the heritage of the site is recommended.
- 6.48 The applicant has provided a Phase 1 and Phase 2 Contaminated Land Report which details the contamination risks on the site. On the basis of these, the Contaminated Land Officer raises no objection subject to a condition regarding the management/approach to any unexpected contamination encountered during the development.
- 6.49 Gatwick Airport have recommended a condition requiring submission and approval of a Bird Hazard Management Plan given the extent of flat/shallow pitched roofs on the buildings which could be attractive to nesting, roosting and loafing birds and therefore a risk to aerodrome safety.

## CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.  
Reason:  
To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.
2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Location Plan	001		31.05.2018
Roof Plan	PL 016		31.05.2018
Roof Plan	PL 015		31.05.2018
Roof Plan	PL 014		31.05.2018
Roof Plan	PL 013		31.05.2018

Elevation Plan	PL 010		31.05.2018
Section Plan	PL 017	A	19.10.2018
Floor Plan	PL 006	A	04.10.2018
Other Plan	TR01	P1	04.10.2018
Floor Plan	PL 003	A	04.10.2018
Floor Plan	PL 005	A	04.10.2018
Floor Plan	PL 008	B	19.10.2018
Elevation Plan	PL 009	A	04.10.2018
Site Layout Plan	PL 002	C	19.10.2018
Arboricultural Plan	PL 12_001	C	04.10.2018
Arboricultural Plan	PL 12_003	C	04.10.2018
Arboricultural Plan	PL 12_002	B	04.10.2018
Elevation Plan	PL 011	A	04.10.2018
Floor Plan	PL 007	A	04.10.2018
Elevation Plan	PL 012	B	19.10.2018
Other Plan	18-033 / 300	T7	01.10.2018
Other Plan	TR05	P1	04.10.2018

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

3. No development shall commence until a Construction Transport Management Plan has been submitted to and approved in writing by the Local Planning Authority. The final plan shall include details of:
  - (a) parking and turning for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) programme of works (including measures for traffic management)
  - (e) provision of boundary hoarding behind any visibility zones
  - (f) construction vehicle routing to and from the site
  - (g) measures to prevent the deposit of materials on the highway
  - (h) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
 has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

4. No development shall commence including demolition or any groundworks preparation until a detailed, scaled Tree Protection Plan (TPP) and the related



Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings. The AMS shall also include a pre commencement meeting with the LPA, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan.

5. No development above ground floor slab level shall commence until the detailed design of the surface water drainage scheme has been submitted to an approved in writing by the Local Planning Authority. Such details should include:
- a) A design that satisfies the SuDS Hierarchy and is compliant with the national non-statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS
  - b) Evidence that the proposed solution will effectively manage the 1 in 30 and 1 in 100 (+40% allowance for climate change) storm events during all stages of the development (pre, post and during), associated discharge rates and storage volumes shall be provided using a Greenfield discharge rate of 30.6l/s (as per the SUDS pro-forma or otherwise agreed by the Local Planning Authority)
  - c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers, etc.)
  - d) Details of how the system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the system is operational
  - e) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected.

Reason:

To ensure that the development is served by an adequate and approved means of drainage which would not increase flood risk on or off site and is suitably maintained throughout its lifetime to comply with Policy Ut4 of the Reigate and Banstead Borough Local Plan 2005, Policy CS10 of the Core Strategy 2014 and the requirements of non-statutory technical standards.

6. No development above ground floor slab level of any part of the development hereby approved shall take place until a Bird Hazard Management Plan detailing how the flat/shallow pitched roofs area will be managed to minimise their attractiveness to birds has been submitted to and approved in writing by the Local Planning Authority.

The Bird Hazard Management Plan shall be implemented upon completion of the roof and shall remain in force for the life of the building and shall not be revised or amended unless otherwise agreed with the Local Planning Authority.

Reason:

To ensure that the roof areas are adequately managed to minimise their attractiveness to birds which could endanger the safe movement of aircraft and the operation of Gatwick Airport with regard to the Town and Country Planning (Safeguarded Aerodromes, Technical Sites and Military Explosives Storage Areas) Direction 2002.

7. The units hereby approved shall be using the external facing materials, including fenestration, balconies and roof, specified on the approved drawings and no others without the prior consent in writing of the Local Planning Authority.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policy Em3

8. All hard and soft landscaping shall be completed in full accordance with the scheme as detailed on the approved Site Plan PL002 Rev C, Landscape General Arrangement and Details Plans (PL12\_001 Rev, PL12\_002 Rev B and PL12\_003 Rev C) prior to occupation or within the first planting season following completion of the development.

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837: Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and the historic gardens in order to comply with policies Pc4 and Em3 of the Reigate and Banstead Borough Local Plan 2005.

9. Notwithstanding the submitted drawings, no part of the development hereby approved shall be first occupied unless and until the following have been constructed or provided in strict accordance with a detailed scheme to be submitted to and approved in writing by the Local Planning Authority:

- a) the existing western most vehicular access from the site to Cross Oak Lane has been constructed and provided within visibility zones of 2.4 metres by 43 metres in both directions; and
- b) double yellow lines on both sides of Cross Oak Lane between the Cross Oak Lane junction with the A23 and the railway bridge to the east of the site.

Reason:

To ensure that the development would not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

10. The development hereby approved shall not be first occupied or brought into use unless and until a scheme for the repair and restoration of the Polar theme statue, and its relocation within the site, has been submitted to and approved in writing by the Local Planning Authority.

Such a scheme should be prepared by a suitably qualified conservator and should include an appraisal of the current condition of the sculpture and the feasibility of, and detailed specification for, any repairs as well as a method statement for returning the statue back to the site, including timeframes.

Reason:

In order to secure a realistic strategy for the preservation of this non-designated heritage asset with regard to policy CS4 of the Reigate and Banstead Core Strategy 2014 and the provisions of the NPPF.

11. The development hereby approved shall not be first occupied or brought into use unless and until the existing eastern most access from the site to Cross Oak Lane has been closed and the kerb, verge and/or footway reinstated in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority.

Reason:

To ensure that the development would not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

12. The development hereby approved shall not be first occupied or brought into use unless and until space has been laid out within the site in accordance with the approved plan numbered PL002 Rev C for vehicles to be parked and for the loading and unloading of HGV vehicles and for all vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking/turning areas, including electric vehicle charging bays, shall be retained and maintained for their designated purposes.

Reason:

To ensure that the development would not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

13. Notwithstanding the submitted Travel Plan, the development hereby approved shall not be first occupied unless and until a revised Travel Plan has been submitted to and approved in writing by the Local Planning Authority. Such a statement should be in accordance with the sustainable development aims and objectives of the National Planning Policy Framework and Surrey County Council's "Travel Plans Good Practice Guide"

The approved Travel Plan shall be implemented upon first occupation and for each subsequent occupation of the development and shall thereafter be maintained and developed to the satisfaction of the Local Planning Authority.

Reason:

To ensure that the development would promote sustainable transport choices with regard to Policy CS17 of the Reigate and Banstead Core Strategy 2014 and in recognition of Section 9 "Promoting Sustainable Transport" in the NPPF.

14. The development shall be carried out in strict accordance with the External Lighting Proposals Report (Issue 2 29 May 2018) by Shepherd Brombley Partnership. No further external lighting other than that expressly identified within the above report shall be installed on site without the prior consent in writing of the Local Planning Authority.

Reason:

To ensure that appropriate external lighting is installed on site in order to safeguard the amenity of neighbouring properties and the character of the area, including the adjoining countryside, from excessive light spill with regard to policy Em3 of the Reigate and Banstead Borough Local Plan 2005.

15. Any contamination not previously identified by the site investigations summarised in the Land Quality Assessment Statement (by Bradbrook Consulting dated 14<sup>th</sup> May 2018), but subsequently found to be present at the site shall be reported to the Local Planning Authority as soon as is practicable.

If deemed necessary development shall cease on site until a remediation method statement, detailing how the unsuspected contamination is to be dealt with, has been submitted to and approved in writing to the Local Planning Authority. The remediation shall thereafter be completed in strict accordance with the approved statement, verification of which shall be provided to the Local Planning Authority.

Reason:

To ensure that the proposed development will not cause harm to human health or pollution of controlled waters with regard to Policy CS10 of the Reigate and Banstead Core Strategy 2014 and the NPPF.

16. The development hereby approved shall not be first occupied unless and until the 4m high acoustic fence along the southern boundary (as shown on approved plan PL002 Rev C) has been erected in accordance with a specification to be submitted to and approved in writing by the Local Planning Authority. Such a specification shall include details of the construction, appearance and acoustic performance of the fence to achieve the sound levels identified in the Noise Impact Assessment (ref PJB8387/18080/V1.1) by Spectrum Acoustic Consultants.

Reason:

To ensure that appropriate external lighting is installed on site in order to safeguard the amenity of neighbouring properties and the character of the area, including the adjoining countryside, from excessive light spill with regard to policy Em3 of the Reigate and Banstead Borough Local Plan 2005.

17. The development hereby approved shall not be first occupied unless and until a verification report demonstrating that the sustainable urban drainage system has been constructed as per the agreed scheme has been submitted to and approved in writing by the Local Planning Authority. The validation report should be carried out by a qualified drainage engineer.

Reason:

To ensure that the development is served by an adequate and approved means of drainage which would not increase flood risk on or off site and is suitably maintained throughout its lifetime to comply with Policy Ut4 of the Reigate and Banstead Borough Local Plan 2005, Policy CS10 of the Reigate and Banstead Core Strategy 2014 and the requirements of non-statutory technical standards.

18. The development shall be carried out and occupied in accordance with the recommendations for mitigation, habitat enhancement and working practices set out in the Preliminary Ecological Appraisal (dated May 2018) and BREEAM Ecology New Construction Assessment (dated May 2018) by Phlorum, including the management plan at Appendix E of the latter document. The recommended bat and bird boxes shall be installed on-site prior to first occupation of any unit on the site.

Reason:

In order to preserve and enhance the wildlife and habitat interest on the site and ensure species present on the site are afforded appropriate protection during construction works with regard to Reigate and Banstead Borough Local Plan 2005 policy Pc2G.

19. Prior to the occupation of any given unit, a Delivery & Servicing Management Plan specific to the occupation and operation of said unit shall be submitted to and approved in writing by the Local Planning Authority. Such a statement should include details of the anticipated number, frequency, type and timing of deliveries and how these will be managed to avoid overspill onto surrounding roads.

The approved Delivery & Servicing Management Plan shall be implemented upon first occupation and shall thereafter be maintained and developed to the satisfaction of the Local Planning Authority, including in the event of a change of occupier.

Reason:

To ensure that the development would promote sustainable transport choices with regard to Policy CS17 of the Reigate and Banstead Core Strategy 2014 and in recognition of Section 9 "Promoting Sustainable Transport" in the NPPF.

20. Prior to the occupation of any given unit, details of any plant or machinery, including fume extraction, ventilation and air conditioning, which may be required, shall be submitted to the Local Planning Authority.

Any plant, machinery or other extraction and ventilation equipment installed on the buildings shall be maintained thereafter in accordance with the approved details and any manufacturer's recommendations.

Reason:

To ensure that a satisfactory external appearance is achieved of the development and to safeguard the amenities of neighbouring occupiers with regard to Reigate and Banstead Borough Local Plan 2005 policy Em3.

21. Within three months of the occupation of each unit hereby approved, a final certificate demonstrating that BREEAM 'Very Good' rating is achieved as a minimum for the unit shall be submitted to and approved by the Local Planning Authority.

Reason:

To ensure that the development is constructed to appropriate sustainability standards with regard to Policy CS11 of the Reigate and Banstead Core Strategy 2014.

22. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015, (or any Order revoking and re-

enacting that Order with or without modification), the units hereby approved shall be occupied for purposes falling within Use Class B1(b), B1(c), B2 or B8 only and shall not be subdivided or used within any other use without the prior written consent of the Local Planning Authority.

Reason:

To control the use of the premises in the interests of maintaining an adequate supply of industrial, storage and distribution uses and with respect to the adequacy of parking provision and potential impacts on the surrounding highway network with regard to policies Em1, Em1A, Em8, Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and policy CS5 of the Reigate and Banstead Core Strategy 2014.

23. No more than 9,098 square metres (gross external area) of floorspace on the development hereby approved shall be used within Use Class B8 at any one time and there shall be no variation of this without the prior written consent of the Local Planning Authority.

Reason:

To control the use of the premises and the potential impacts on the surrounding highway network with regard to policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and policy CS5 of the Reigate and Banstead Core Strategy 2014.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is advised that prior to the occupation of the development, adequate provision should be made for waste storage and collection. You are advised to contact the Council's Recycling and Cleansing team to discuss the required number and specification of wheeled bins on [rc@reigate-banstead.gov.uk](mailto:rc@reigate-banstead.gov.uk) or on the Council's website at [http://www.reigate-banstead.gov.uk/info/20051/commercial\\_waste](http://www.reigate-banstead.gov.uk/info/20051/commercial_waste).
3. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;

- (f) Only minimal security lighting should be used outside the hours stated above; and
- (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit. In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

4. The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see: [www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme](http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme). The applicant is also advised that consent may be required under Section 23 of the Land Drainage Act 1991. Please see: [www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice](http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice)
5. When a temporary access is approved or an access is to be closed as a condition of planning permission an agreement with, or licence issued by, the Highway Authority Local Highways Service will require that the redundant dropped kerb be raised and any verge or footway crossing be reinstated to conform with the existing adjoining surfaces at the developers expense.
6. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
7. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.
8. The developer is advised that as part of the detailed design of the highway works required by the above condition(s), the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
9. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly

loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

10. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
11. With respect to the Travel Plan required by the above conditions, the following advice is provided to the developer in respect of the revisions and information required:
  - (a) Information on the likely quantum of employees working on the site.
  - (b) Clarification is needed on whether “operator” means “site management company
  - (c) As each unit becomes occupied, the occupier should then complete a pro-forma and submit it to the TPC (this wording could be added to para 6.2.7).
  - (d) Contact details of the Travel Plan Coordinator
  - (e) The action plan states the cycle parking, lockers and showers will be installed by the occupier, however, I would expect all of these facilities to be installed by the developer, otherwise there will be a need to retro-fit. Although there is an action plan, there is also a list of proposed measures in Table 6.1 as well as a description of other measures in Section 6. The content of all these all differ slightly, which is confusing. For example, 6.3.6 refers to an annual newsletter, 6.7.2 refers to the TPC seeking cycle shop discounts etc. These 2 measures haven’t been included in Table 6.1 or Table 8.1. It would be clearer to have one action plan which includes all measures; the detailed description of these measures can still be retained, but all measures should appear in the action plan.
  - (f) Different organisations are likely to occupy the units. And there is a risk that the units won’t be fully occupied for some time. Therefore, a survey should be undertaken within 3 months of occupation, regardless of occupancy in order to obtain base line data.
  - (g) No specific Single Occupancy Vehicle target is proposed. “Driving a car or van” is listed in Table 5.2, but this should be split into “driving alone” and “car sharing”.
  - (h) Interim targets have been identified for Yrs 3 + 5. However, Year1 targets are also required.
  - (i) Wording should also be included to state that the resulting monitoring reports will be submitted to SCC and to the LPA.
12. The developer would be expected to instruct an independent transportation data collection company to undertake the monitoring survey. This survey should conform to a TRICS Multi-Modal Survey format consistent with the UK Standard for Measuring Travel Plan Impacts as approved by the Highway Authority. To ensure that the survey represents typical travel patterns, the organisation taking ownership of the travel plan will need to agree to being surveyed only within a specified annual quarter period but with no further notice of the precise survey dates. The Developer would be expected to fund the survey validation and data entry costs.



13. The developer is advised that as part of the detailed design of the highway works required by the above condition(s), the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
14. If the proposed works affect an Ordinary Watercourse, Surrey County Council as the Lead Local Flood Authority should be contacted to obtain prior written consent.
15. The developer is reminded that, as a riparian owner of the Cross Oak Lane ditch, it is their responsibility to ensure that the watercourse is kept in a good condition and free from blockages.
16. Any works within 8m of the ditch/river will require a Flood Risk Activity Permit. For further information, please see [www.gov.uk/guidance/flood-risk-activities-environmental-permits](http://www.gov.uk/guidance/flood-risk-activities-environmental-permits)
17. The developer is reminded of the need to comply with Network Rail requirements and standards for the safe operation of the railway and the protection of Network Rail's adjoining land, both during construction and after completion of works. In particular, the developer must ensure that, both during construction and after completion of the works, the proposal does not encroach onto Network Rail land, affect the safety, operation or integrity of the company's railway and its infrastructure, undermine its support zone, damage the company's infrastructure, place additional load on cuttings, adversely affect any railway land or structure, over-sail or encroach upon the air space of any Network Rail land or cause to obstruct or interfere with any works or proposed works or Network Rail development both now and in the future. Network Rail strongly recommends that the developer contacts Network Rail Asset Protection London South East at [assetprotectionsussex@networkrail.co.uk](mailto:assetprotectionsussex@networkrail.co.uk) prior to any works commencing on site.
18. The use of a suitably qualified arboricultural consultant is essential to provide acceptable supervision and monitoring in respect of the arboricultural issues in respect of the above condition. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
19. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and shrubs shall be in keeping with the character and appearance of the locality.
20. The applicant's attention is drawn to the specifics of the contaminated land conditional wording such as 'prior to commencement', 'prior to occupation' and 'provide a minimum of two weeks' notice'. The submission of information not in accordance with the specifics of the planning conditional wording can lead to delays in discharging conditions, potentially result in conditions being unable to be discharged or even enforcement action should the required level of evidence/information be unable to be supplied. All relevant information should be formally submitted to the Local Planning Authority and not direct to Environmental Health.

21. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings.

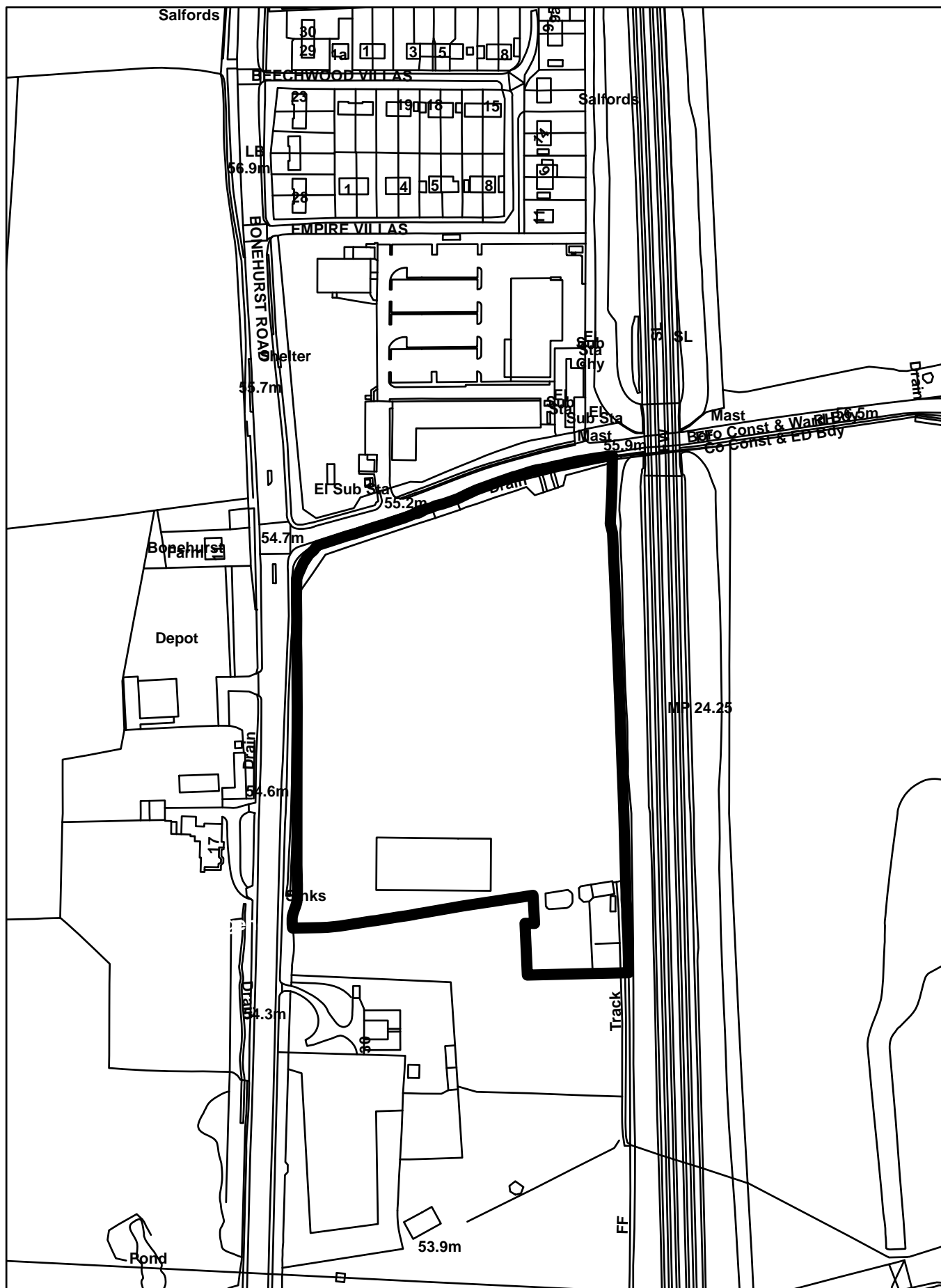
If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found [http://www.reigate-banstead.gov.uk/info/20277/street\\_naming\\_and\\_numbering](http://www.reigate-banstead.gov.uk/info/20277/street_naming_and_numbering)

### **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies CS1, CS4, CS5, CS8, CS10, CS11, CS12, CS17, Em1, Em1A, Em2, Em3, Em8, Mo3, Mo4, Mo5, Mo6, Mo7 and Ut4 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

# 18/01180/F - Former Philips Research Laboratories South Site, Crossoak Lane, Salfords







N

Figured dimensions only are to be used. All dimensions to be checked onsite. Differences between drawings and between drawings and specification or bills of quantities to be reported to the PRC Group.

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Revisions:

Drawn/Chkd: Date:

A. Plan revised in accordance with planners comments. BDS Sept 18

B. Landscaping and parking amended to Unit 5 in accordance with planners comments. AMc Sept 18

Unit	GEA			CIH (m)
	Ground	First	Total	
1	26,136 ft² / 2,428 m²	4,671 ft² / 434 m²	30,807 ft² / 2,862 m²	10.5
2	22,310 ft² / 2,073 m²	3,475 ft² / 323 m²	25,785 ft² / 2,396 m²	10.5
3	17,910 ft² / 1,664 m²	2,785 ft² / 259 m²	20,695 ft² / 1,923 m²	10.5
4	42,723 ft² / 3,969m²	6,815 ft² / 633 m²	49,538 ft² / 4,602 m²	12.5
5	35,553 ft² / 3,301 m²	5,801 ft² / 539 m²	41,354 ft² / 3,840m²	12.5
T	144,632 ft² / 13,435 m²	23,547 ft² / 2,188 m²	168,179 ft² / 15,623m²	

SITE AREA - 7.71A / 3.12 HA  
DEVELOPMENT DENSITY (GIA) - 48.8%

KEY

Site application boundary

2.4m high weld mesh fence

4m acoustic fence

External cycle shelter for 10 no. cycle parking spaces

External cycle shelter for 14 no. cycle parking spaces

• EV

Electric vehicle charging point (x2 vehicles per post)

• B

Steel bollards (Green RAL 6018)

DB

Directory Board

GAS

Gas kiosk (Anthracite RAL 7016)

Block paving (vehicular areas) (Silver grey colour)

Block paving (pedestrian areas) (Charcoal colour)

Concrete service yard

Tarmacadam surfacing

Well consolidated gravel with fines

Soft landscaping

Native screening

Main river

Paving slabs

TP

Tactile paving

Existing trees

Proposed trees

S

Relocated sculpture

Client:

HE2 N1 SALFORDS 1 GP LIMITED

Project:

CROSS OAK LANE, SALFORDS

Drawing Title:

PROPOSED SITE PLAN

Scale @ A1:

1:500

Checked by:

AMc

Date:

Apr 18

Job No:

10947

Stage:

PL 002

Drawing No:

Rev:

B

Issue Status:

Construction ☐ Preliminary ☐

Information ☐ Approval ☒

Tender ☐

Architecture

Planning

Master Planning

Urban Design

Interiors

Landscape

Offices

Woking

London

Milton Keynes

Warsaw



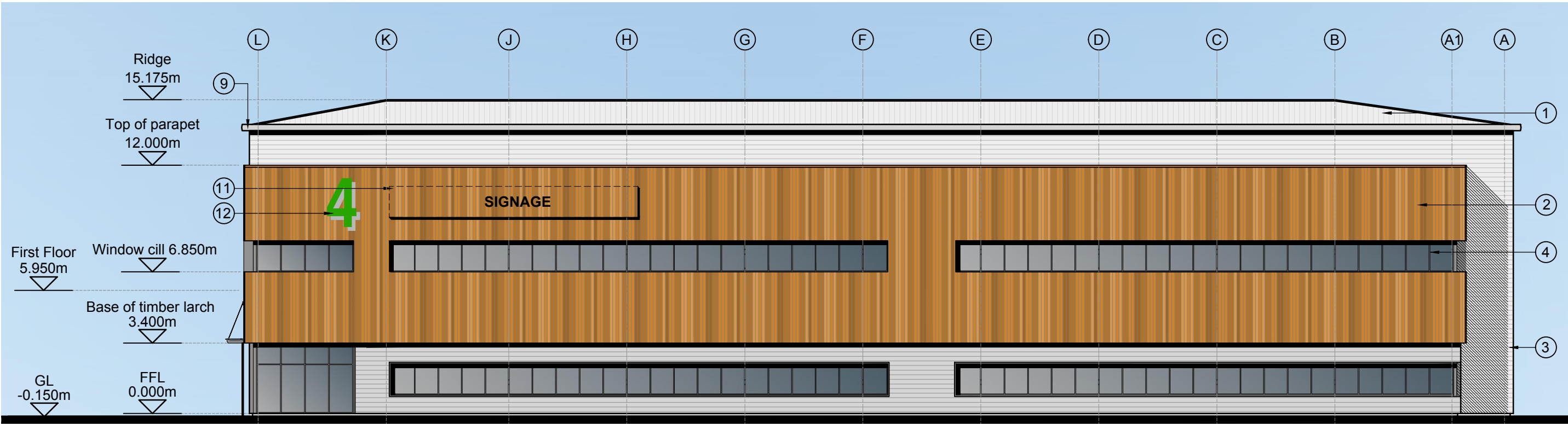


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Revisions: Drawn/Chkd: Date: HB SEP 18

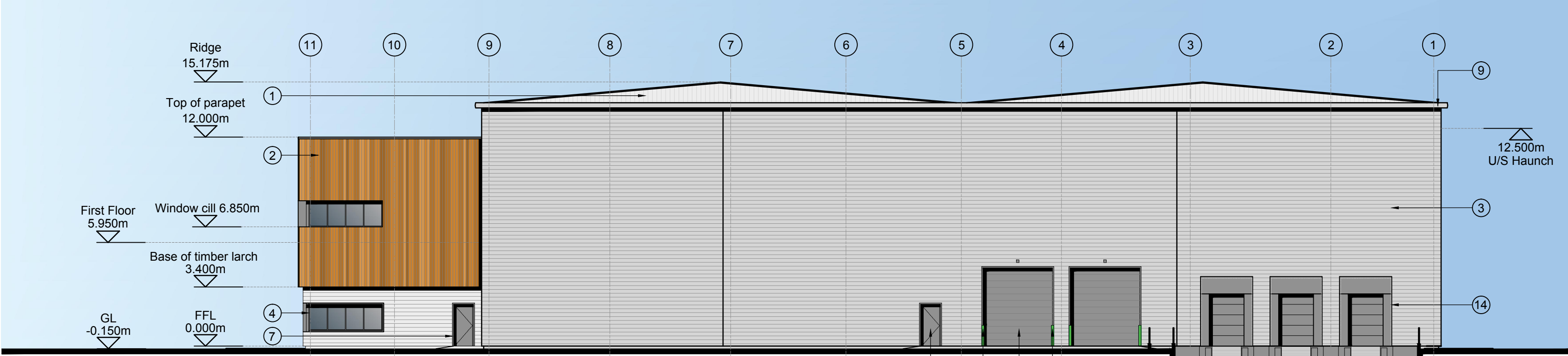
A. Changes made in accordance with planners comments. Building areas adjusted accordingly.



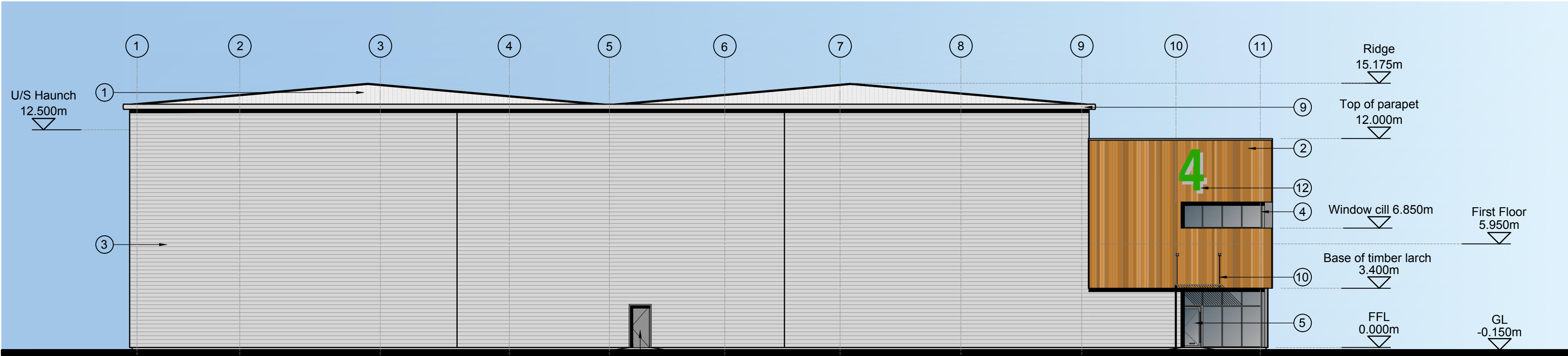
WEST ELEVATION



EAST ELEVATION

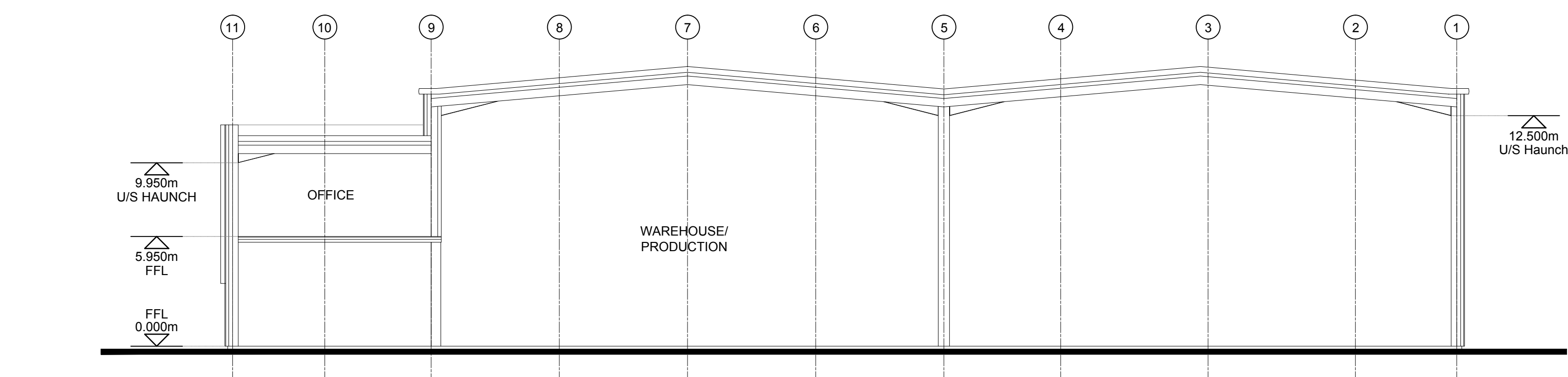


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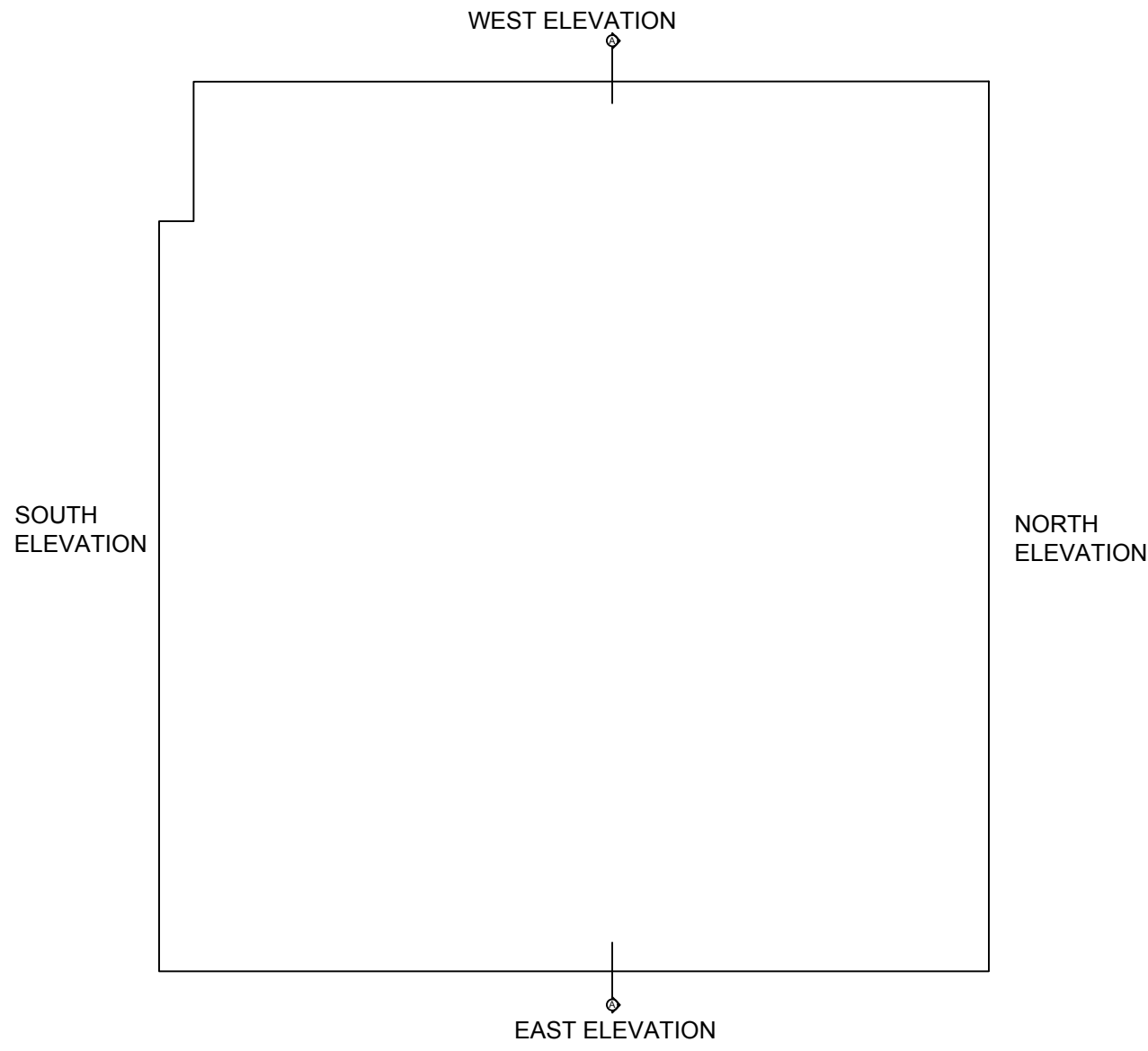
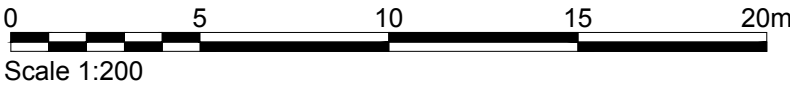


NORTH ELEVATION

- MATERIALS KEY**
- 1 Profilled roof cladding (Goosewing grey RAL 7038)
  - 2 Siberian larch cladding
  - 3 Horizontal arc profile cladding (Metallic silver RAL 9006)
  - 4 Aluminium framed windows (Anthracite RAL 7016)
  - 5 Aluminium framed entrance doors (Anthracite RAL 7016)
  - 6 Loading bay doors (Anthracite RAL 7016)
  - 7 Steel personnel doors (Anthracite RAL 7016)
  - 8 Steel bollard (Green RAL 6018)
  - 9 Aluminium rainwater goods (Metallic silver RAL 9006)
  - 10 Glazed canopy with gravity fed rainwater pipe
  - 11 Signage zone by others
  - 12 Backlit illuminated aluminium unit numeral (RAL 6018 face)
  - 13 Ceramic backed glazing infill panel
  - 14 Dock leveller (Anthracite RAL 7016)



SECTION A - A



REFERENCE PLAN

Client: HE2 N1 SALFORDS 1 GP LIMITED



Project: CROSS OAK LANE, SALFORDS

12 Warren Yard,  
Warren Park,  
Milton Keynes,  
MK12 5NW  
01908 305 246

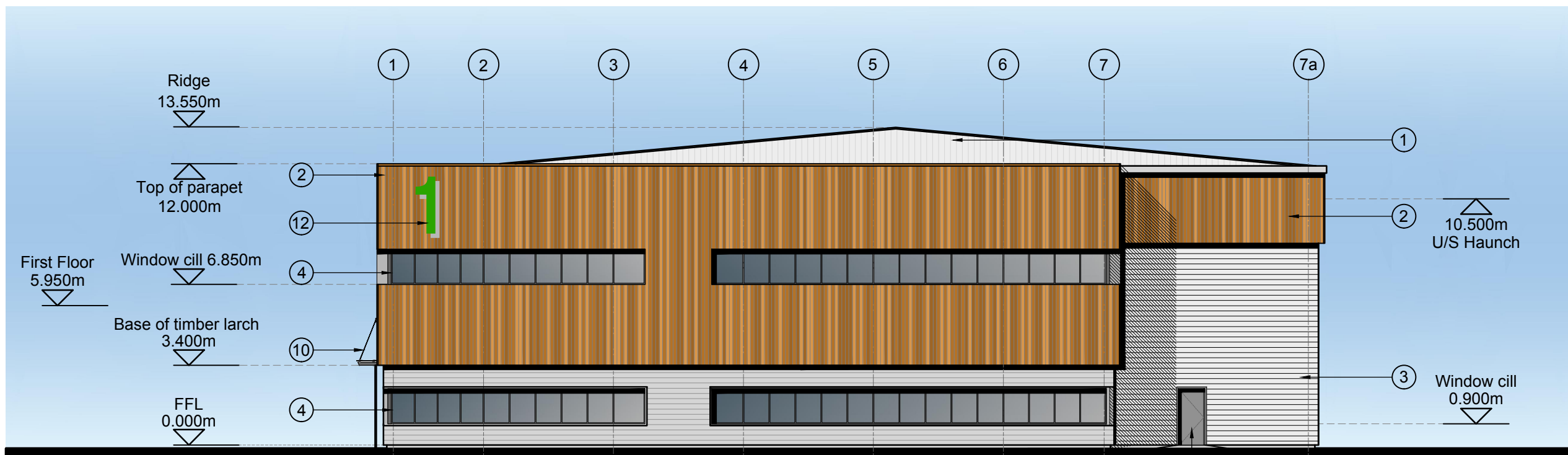
info@prc-group.com  
www.prc-group.com

Drawing Title: UNIT 4 PROPOSED ELEVATIONS

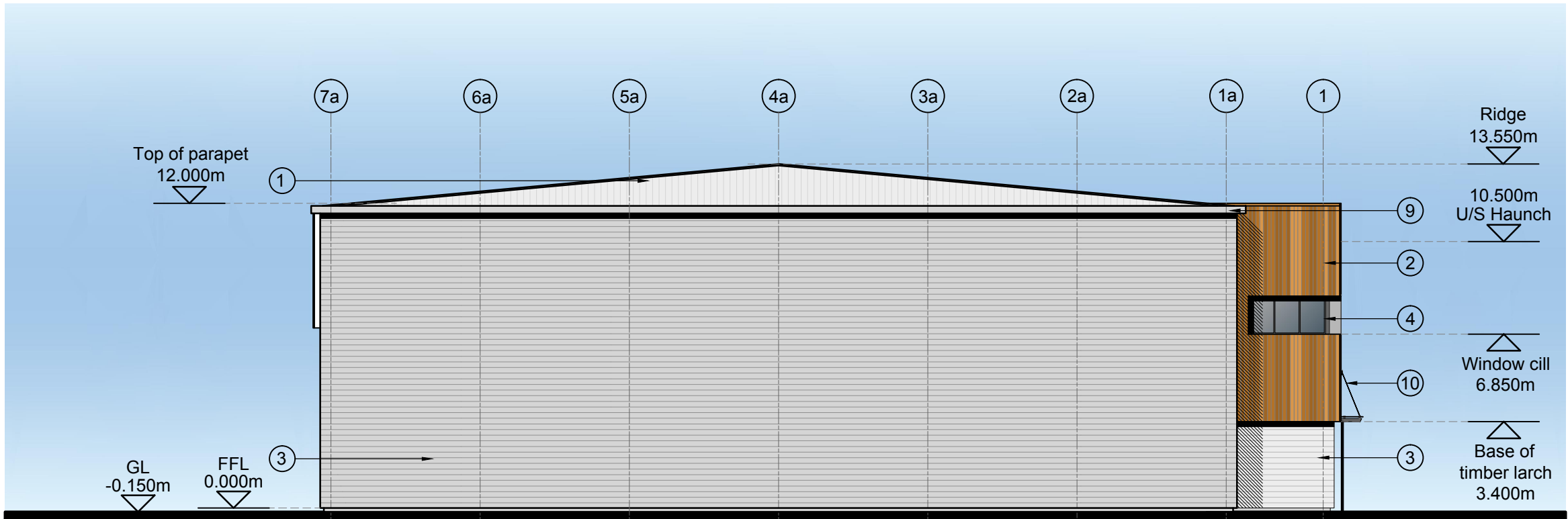
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Checked by: AMc  
Date: SEP 18  
Job No: 10947  
Stage: Drawing No: PL 011  
Rev: A

Issue Status:  
Construction ☐ Preliminary ☐  
Information ☐ Approval ☒  
Tender ☐  
Offices:  
Working  
London  
Milton Keynes  
Warsaw

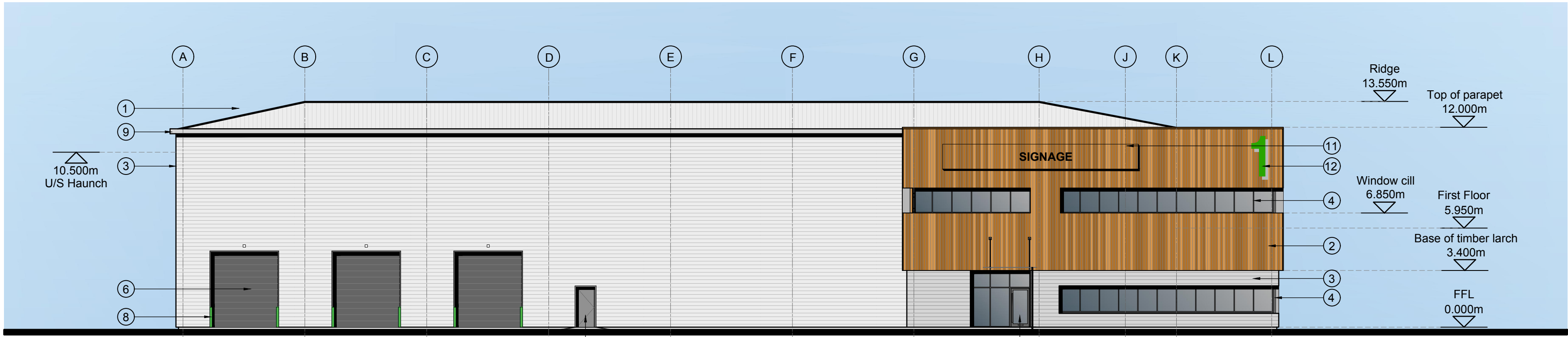
PRC Architecture & Planning



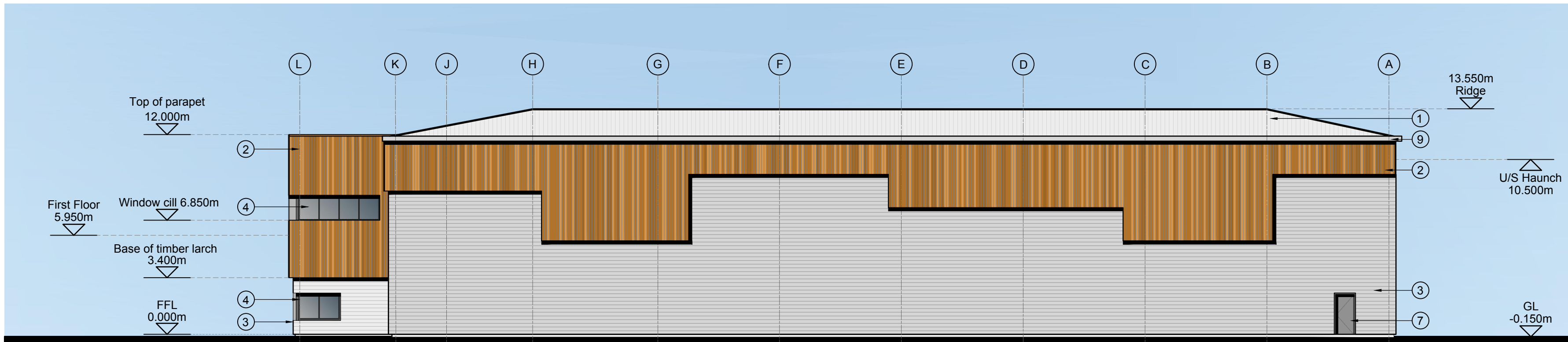
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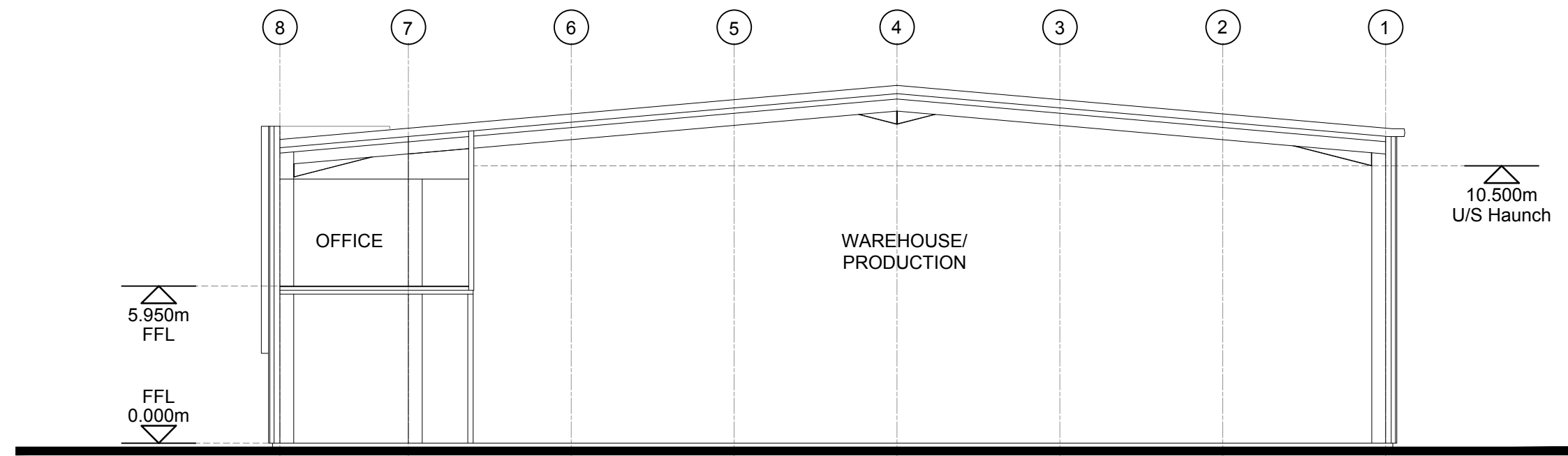
SOUTH ELEVATION



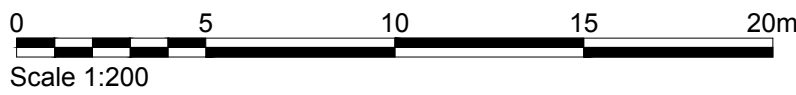
EAST ELEVATION



WEST ELEVATION



SECTION A - A



MATERIALS KEY

- ① Profiled roof cladding (Goosewing grey RAL 7038)
- ② Siberian larch cladding
- ③ Horizontal arc profile cladding (Metallic silver RAL 9006)
- ④ Aluminium framed windows (Anthracite RAL 7016)
- ⑤ Aluminium framed entrance doors (Anthracite RAL 7016)
- ⑥ Loading bay doors (Anthracite RAL 7016)
- ⑦ Steel personnel doors (Anthracite RAL 7016)
- ⑧ Steel bollard (Green RAL 6018)
- ⑨ Aluminium rainwater goods (Metallic silver RAL 9006)
- ⑩ Glazed canopy with gravity fed rainwater pipe
- ⑪ Signage zone by others
- ⑫ Backlit illuminated aluminium unit numeral (RAL 6018 face)
- ⑬ Ceramic backed glazing infill panel
- ⑭ Dock leveller (Anthracite RAL 7016)

Figured dimensions only are to be used. All dimensions to be checked onsite. Differences between drawings and between drawings and specification or bills of quantities to be reported to the PRC Group.

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Revisions: Drawn/Chkd: Date: HB SEP 18

A. Changes made in accordance with planners comments. Building areas adjusted accordingly.

Client: HE2 N1 SALFORDS 1 GP LIMITED



Project: CROSS OAK LANE, SALFORDS

12 Warren Yard,  
Warren Park,  
Milton Keynes,  
MK12 5HW  
01908 305 246

info@prc-group.com  
www.prc-group.com

Drawing Title: UNIT 1 PROPOSED ELEVATIONS

Scale @ A1: 1:200 Checked by: AMc Date: SEP 18

Job No: 10947 Stage: PL Drawing No: 009 Rev: A

Issue Status: Construction ☐ Preliminary ☐ Information ☐ Approval ☒ Tender ☐

Offices: Working London Milton Keynes Warsaw

PRC Architecture & Planning

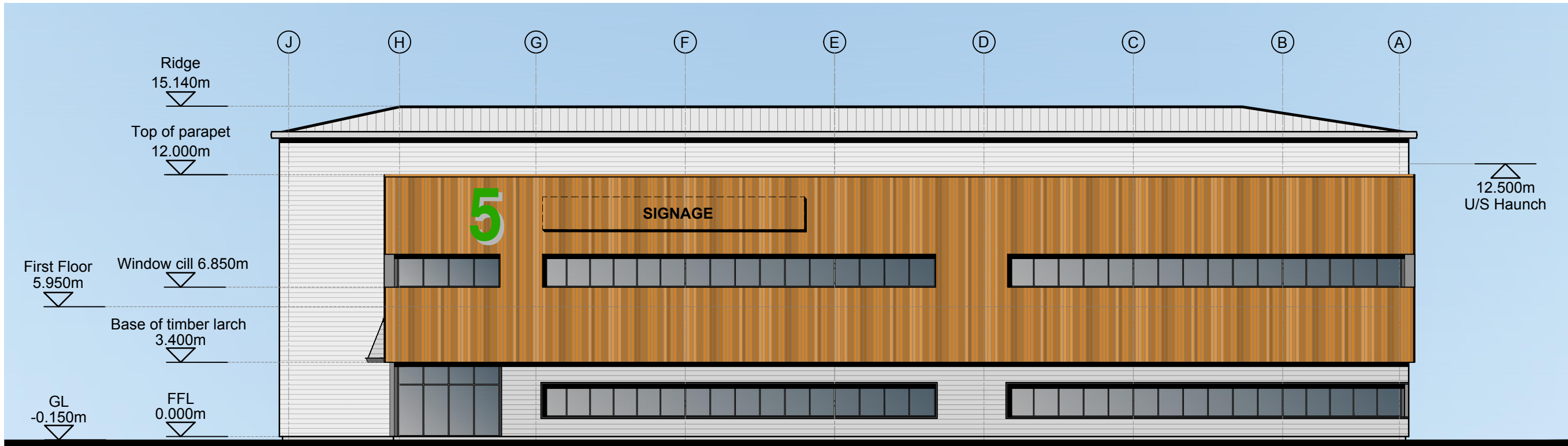


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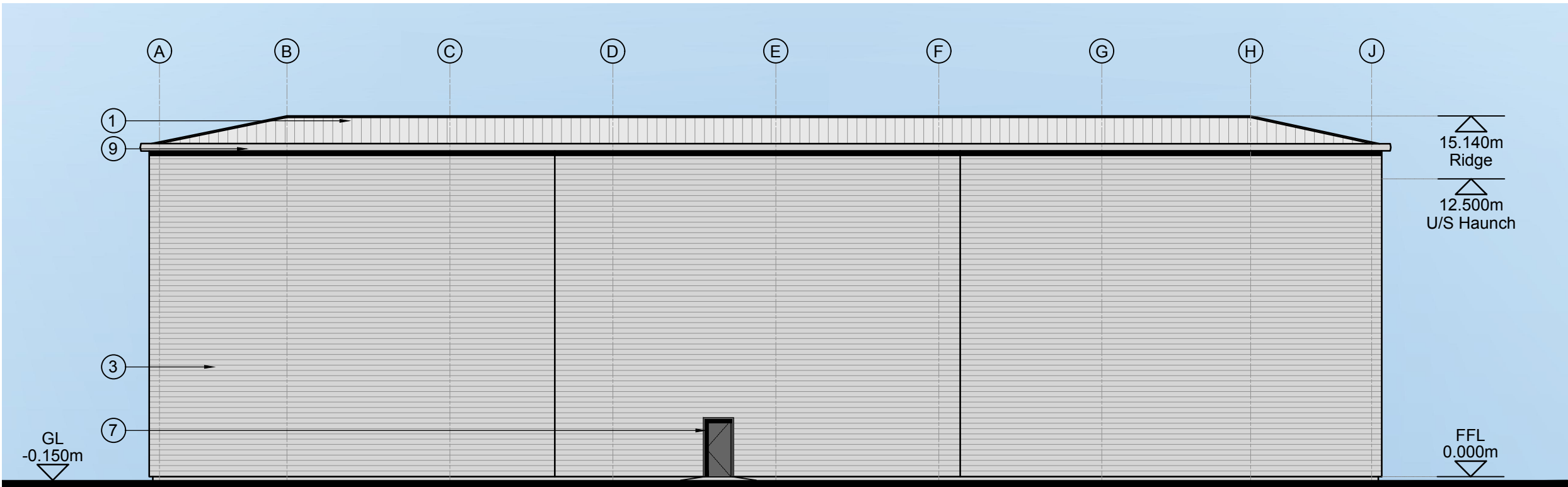
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Revisions: Drawn/Chkd: Date:

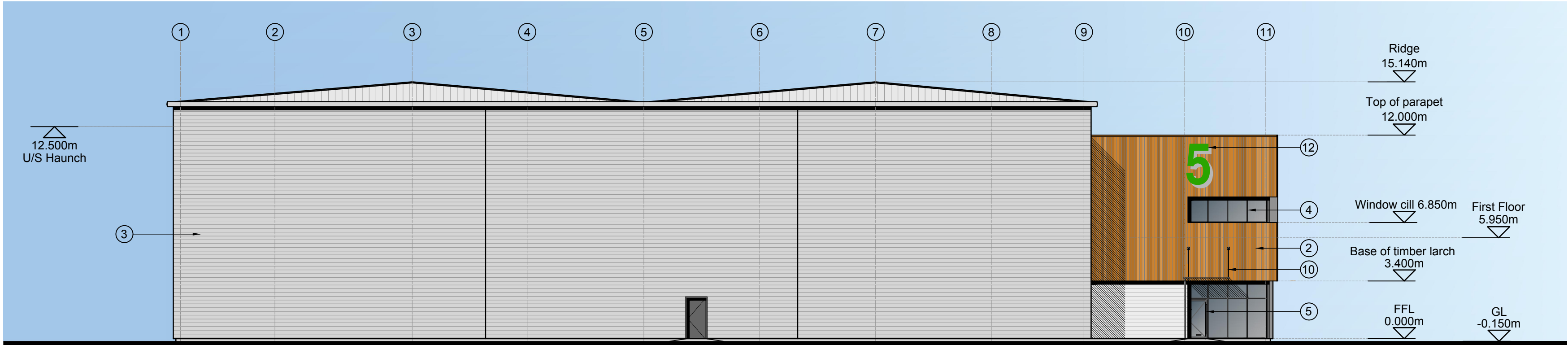
A. Changes made in accordance with planners comments. Building areas adjusted accordingly. HB SEP 18



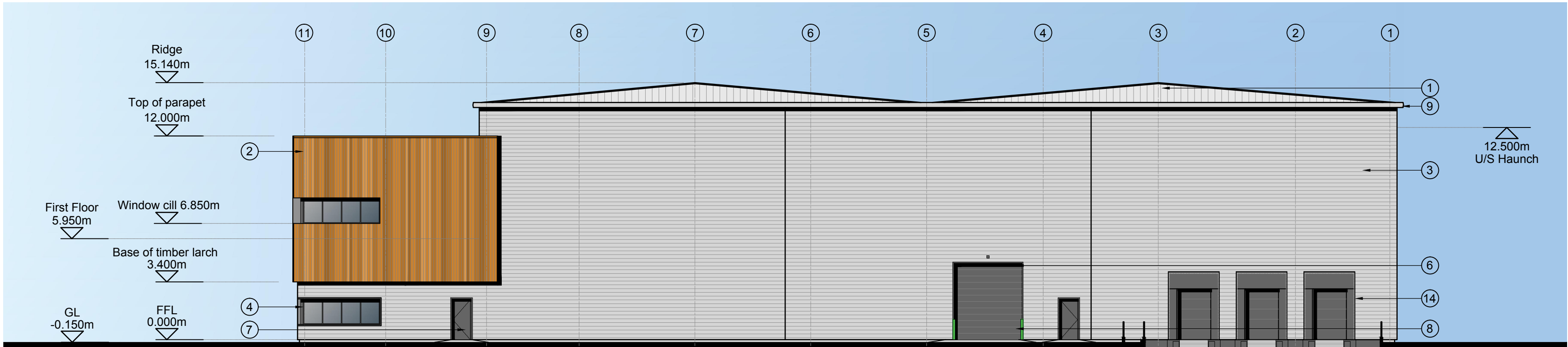
WEST ELEVATION



EAST ELEVATION



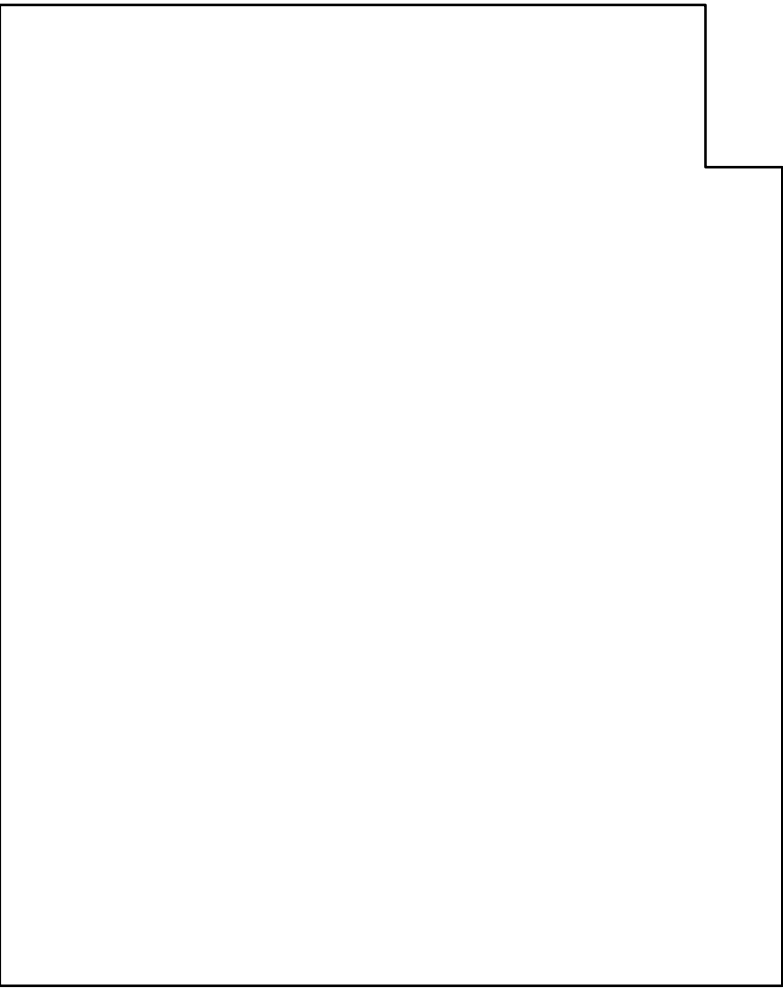
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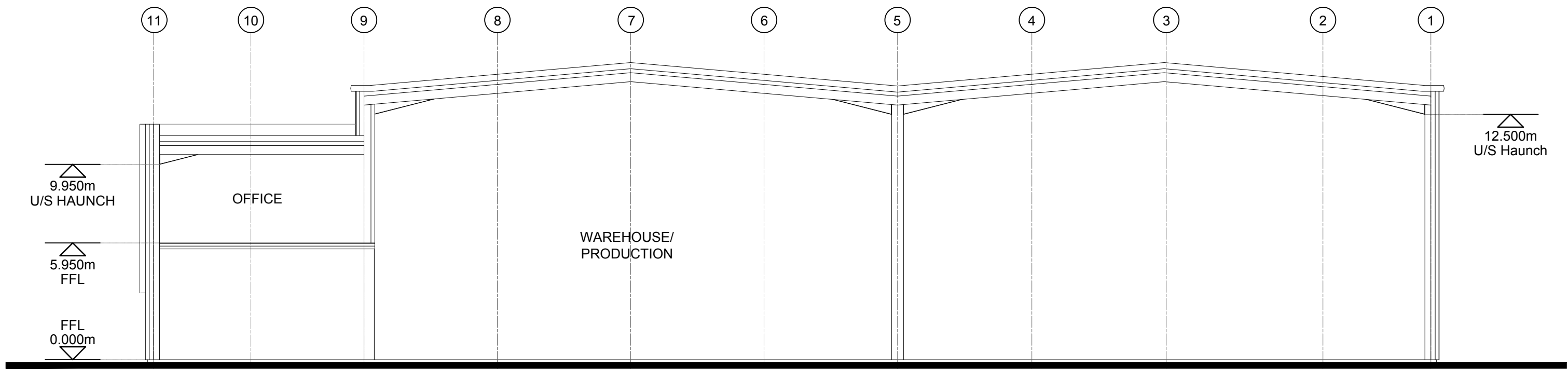
SOUTH ELEVATION

- MATERIALS KEY**
- ① Profiled roof cladding (Goosewing grey RAL 7038)
  - ② Siberian larch cladding
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  - ⑪ Signage zone by others
  - ⑫ Backlit illuminated aluminium unit numeral (RAL 6018 face)
  - ⑬ Ceramic backed glazing infill panel
  - ⑭ Dock leveller (Anthracite RAL 7016)

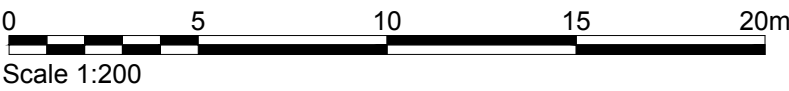
WEST ELEVATION



REFERENCE PLAN



SECTION A - A



Client: HE2 N1 SALFORDS 1 GP LIMITED



Project: CROSS OAK LANE, SALFORDS

12 Warren Yard,  
Warren Park,  
Milton Keynes,  
MK12 5HW  
01908 305 246

info@prc-group.com  
www.prc-group.com

Drawing Title: UNIT 5 PROPOSED ELEVATIONS

Scale @ A1: 1:200 Checked by: AMc Date: MAY 18

Job No: 10947 Stage: Drawing No: PL 012 Rev:

Issue Status: Construction ☐ Preliminary ☐ Information ☐ Approval ☒ Tender ☐

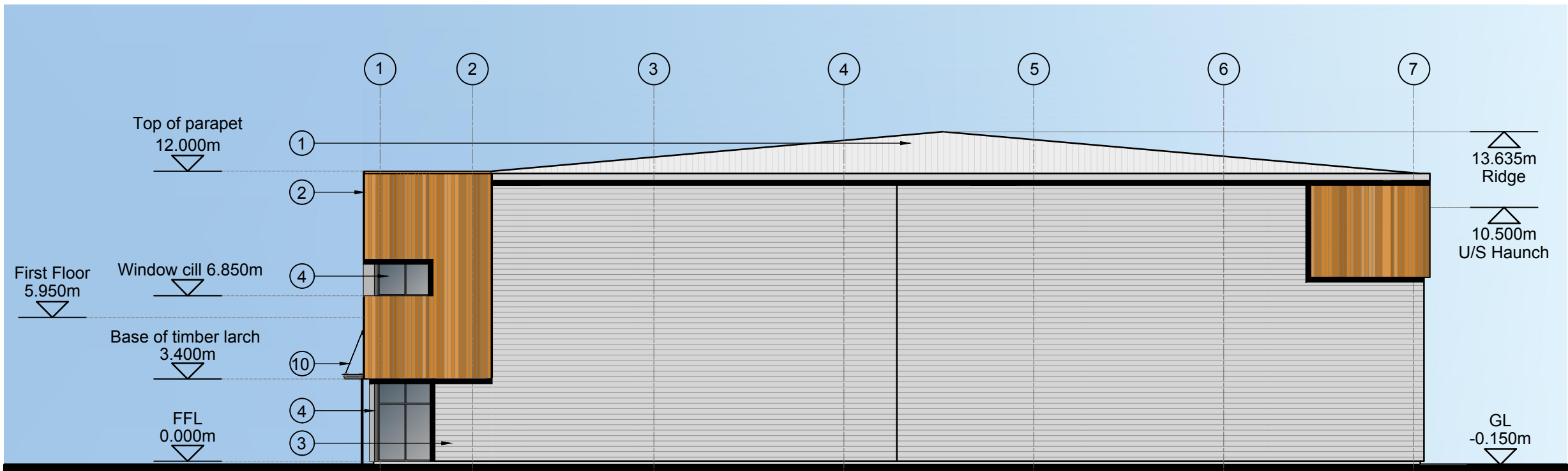
Offices: Working London Milton Keynes Warsaw

PRC Architecture & Planning

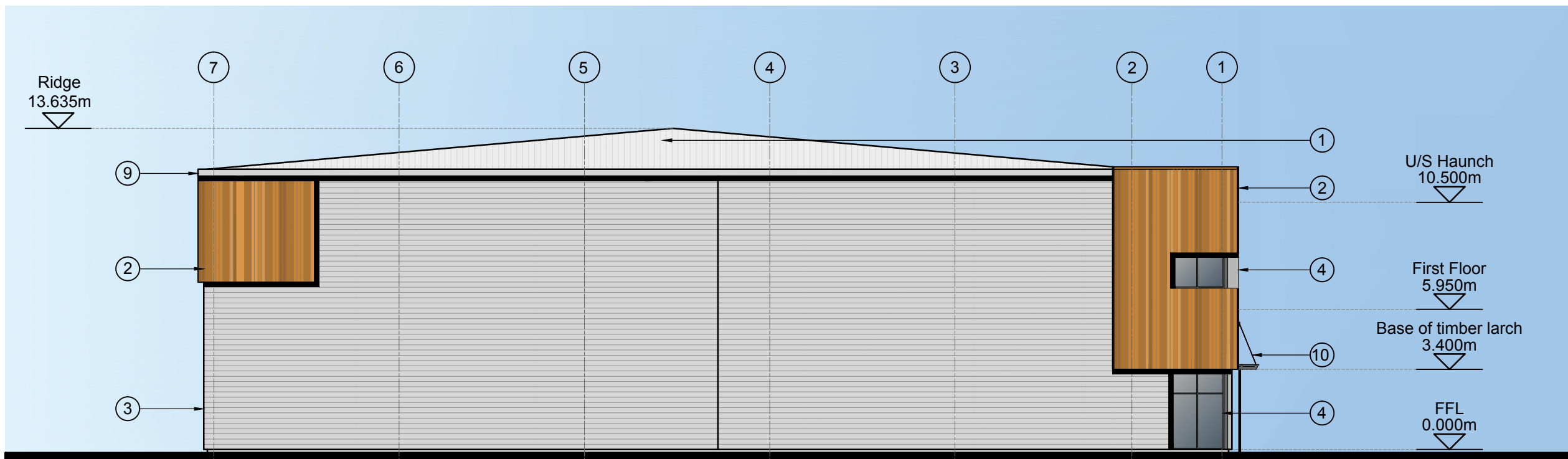
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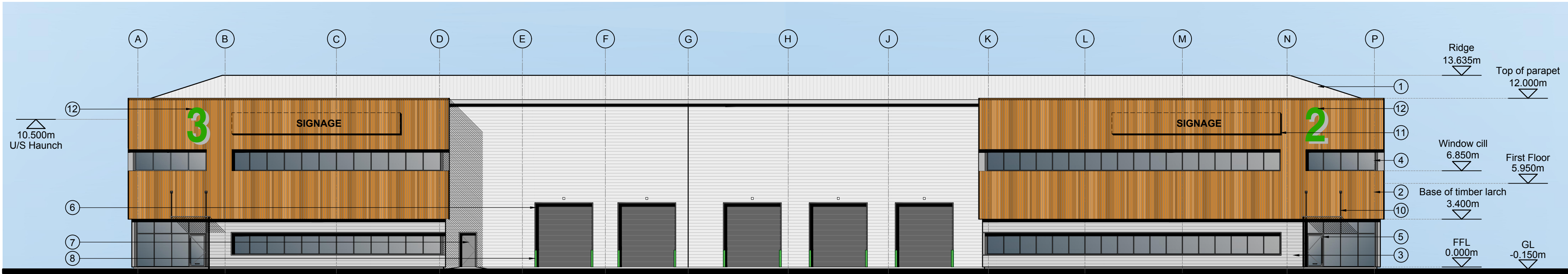
Revisions: Drawn/Chkd: Date:



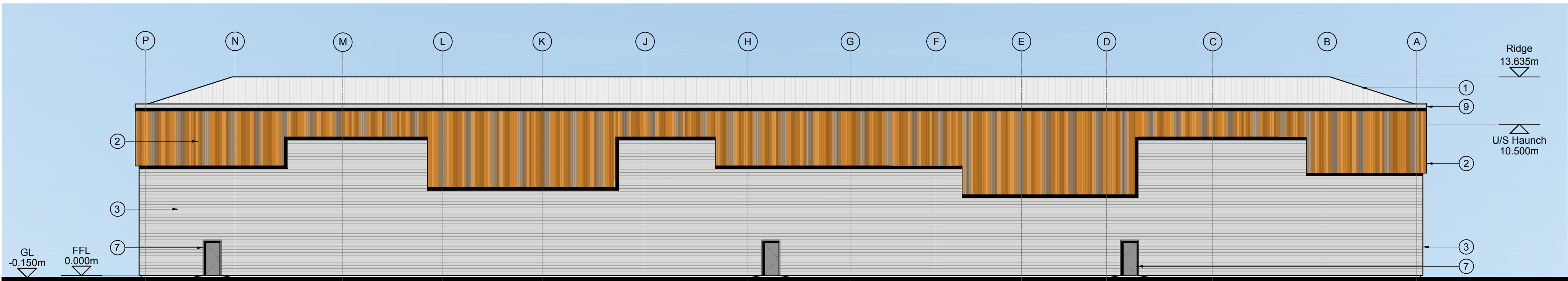
NORTH ELEVATION



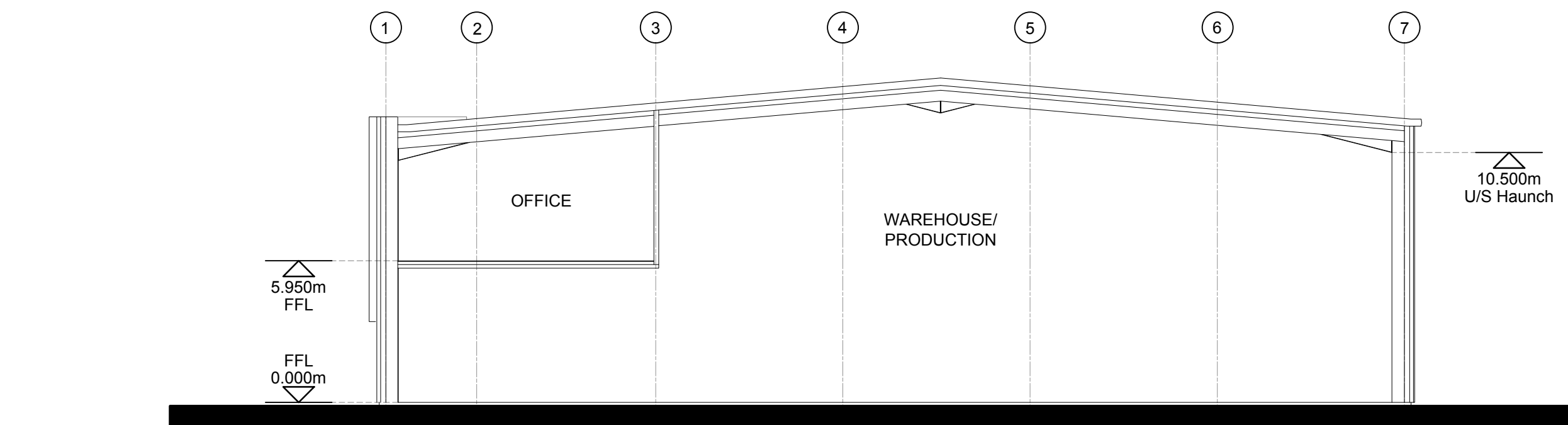
SOUTH ELEVATION



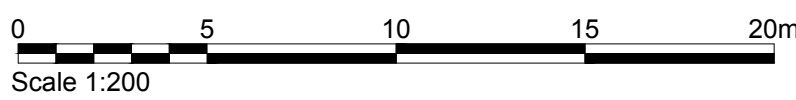
EAST ELEVATION



WEST ELEVATION



SECTION A - A



MATERIALS KEY

- 1. Profiled roof cladding (Goosewing grey RAL 7038)
- 2. Siberian larch cladding
- 3. Horizontal arc profile cladding (Metallic silver RAL 9006)
- 4. Aluminium framed windows (Anthracite RAL 7016)
- 5. Aluminium framed entrance doors (Anthracite RAL 7016)
- 6. Loading bay doors (Anthracite RAL 7016)
- 7. Steel personnel doors (Anthracite RAL 7016)
- 8. Steel bollard (Green RAL 6018)
- 9. Aluminium rainwater goods (Metallic silver RAL 9006)
- 10. Glazed canopy with gravity fed rainwater pipe
- 11. Signage zone by others
- 12. Backlit illuminated aluminium unit numeral (RAL 6018 face)
- 13. Ceramic backed glazing infill panel
- 14. Dock leveller (Anthracite RAL 7016)



REFERENCE PLAN

Client: HE2 N1 SALFORDS 1 GP LIMITED

Project: CROSS OAK LANE, SALFORDS

Drawing Title: UNITS 2 - 3 PROPOSED ELEVATIONS

Scale @ A1: 1:200 Checked by: AMC Date: MAY 18

Job No: 10947 Stage: Drawing No: PL 010 Rev:

Issue Status: Construction Information Tender Preliminary Approval Working London Milton Keynes Warsaw

PRC Architecture & Planning



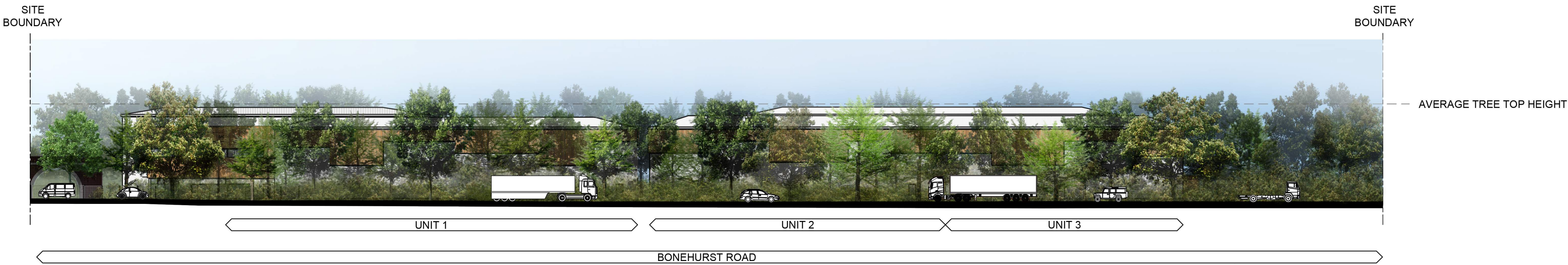
12 Warren Yard, Warren Park, Milton Keynes, MK12 5NW 01908 305 246

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Architecture Planning Master Planning Urban Design Interiors Landscape

Offices Working London Milton Keynes Warsaw





SECTION D - D



SECTION B - B



SECTION C - C

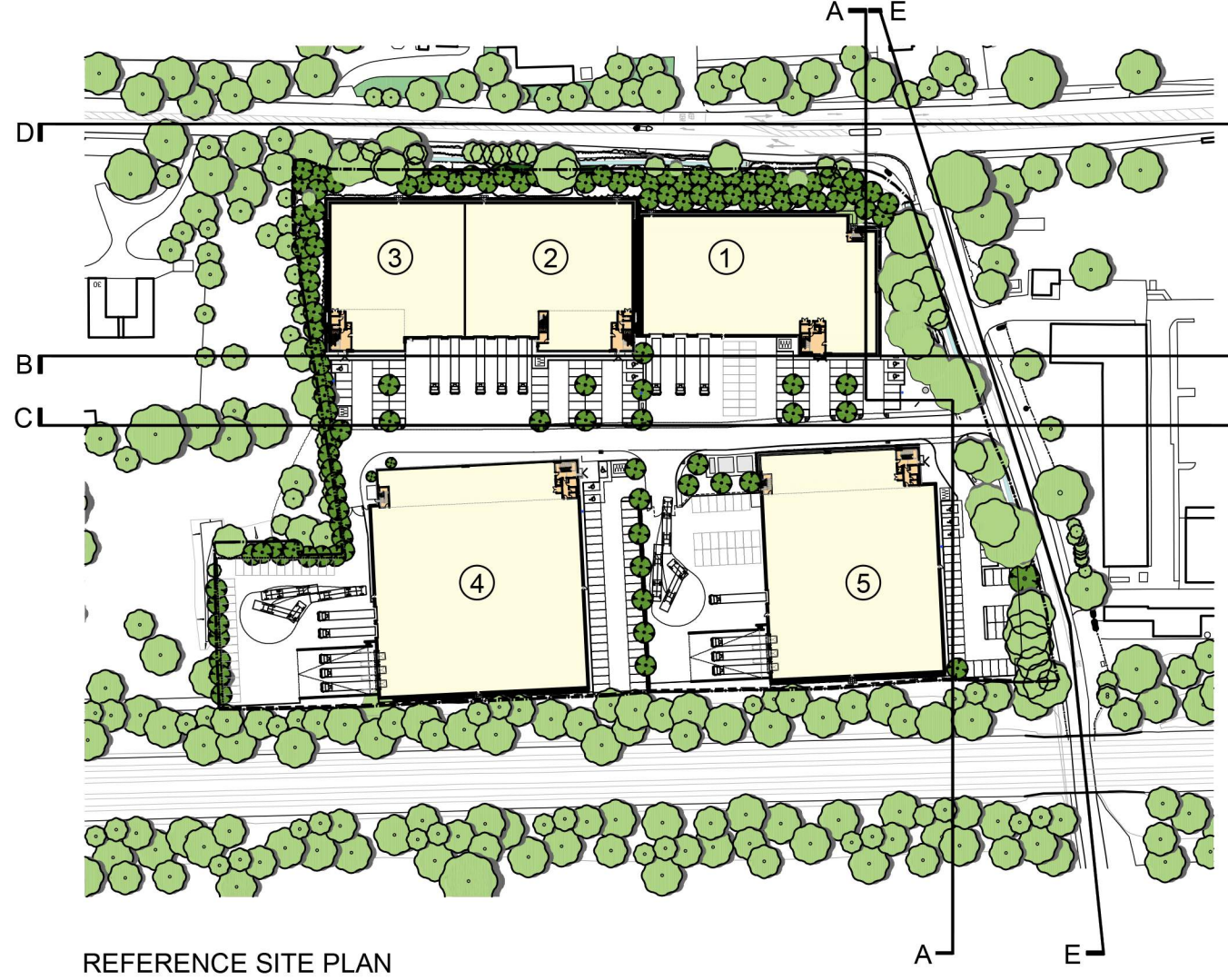


SECTION E - E



SECTION A - A

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Scale 1:500



Client:  
HE2 N1 SALFORDS 1 GP LIMITED

Project:  
CROSS OAK LANE, SALFORDS

Drawing Title:  
SITE SECTIONS

Scale @ A1: 1:500  
Checked by: AMC  
Date: MAY 18  
Job No: 10947  
Stage: Drawing No: PL 017  
Rev:

Issue Status:  
Construction ☐ Preliminary ☐  
Information ☐ Approval ☐  
Tender ☐

PRC Architecture & Planning



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Woking  
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Milton Keynes  
Warsaw